



WEST COLONIAL DRIVE CORRIDOR STUDY

City of Orlando October 8, 2025

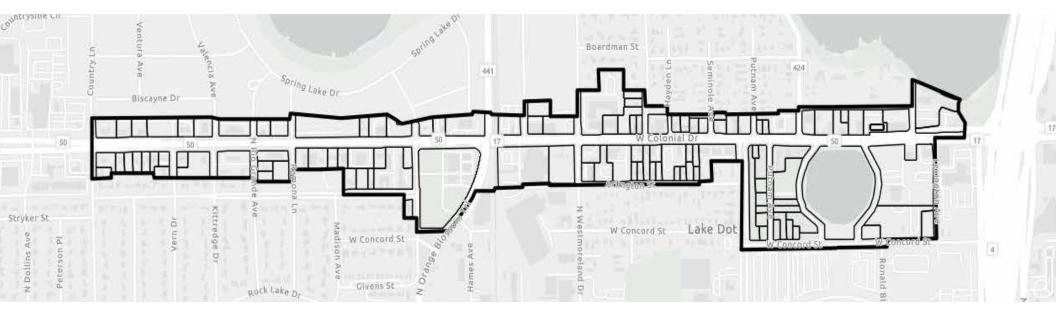




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INTRODUCTION

The purpose of this study is to encourage redevelopment along the West Colonial Drive Corridor and identify needed standards to require compatibility with adjacent neighborhoods. The corridor includes several blocks with plentiful street trees and established office uses with an Annual Average Daily Traffic volume of 61,500 vehicles. Retail spaces face more challenges, and there are a number of vacant lots. With the success of Creative Village and the Packing

District, completion of nearby I-4 construction, and entertainment venues of Inter & Co Stadium, Camping World Stadium, and Kia Center, this area may have expanded opportunity for redevelopment in coming years. The City wants to ensure that new development is compatible with the context and complements the rich historic character of nearby neighborhoods including Lake Dot, Rock Lake (including Spring Lake Manor), Spring Lake and College Park.



Figure 1 Illustrates the study area as well as the surrounding neighborhoods and waterbodies.

The study area overlaps a number of improvement oriented districts including:

- Community Redevelopment Agency (CRA) Downtown Orlando
- Orange Blossom Trail CRA
- Federally-designated Opportunity Zones
- Orlando Economic Enhancement District (OEED)
- Urban Job Tax Credit Program
- Two Main Street Districts under the Main Streets Program:
 - West Lakes Market Street District
 - Parramore Main Street





the federally designated Opportunity Zone and OEED within the study area. Figure 3 (above) illustrates the Urban Tax Credit Program, and Main **Street Programs: West Lakes Market Street District and Parramore Main Street. These** maps were developed for the City of Orlando's Vision **Zero: Connecting Residents** on Safe Streets (CROSS)

https://www.orlando.gov/ **Initiatives/Vision-Zero/CROSS** The Study Area includes the properties on or near West Colonial Drive from I-4 to Tampa Avenue. The study evaluates existing conditions to identify the strengths and weaknesses of development along the corridor. The results of this analysis, in conjunction with community input, inform the recommendations about future development in order to encourage investment in the community.

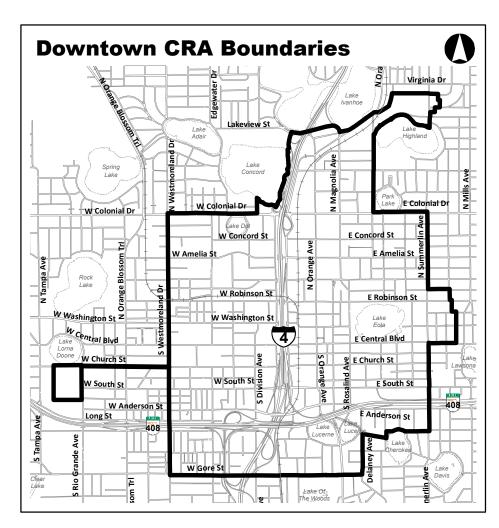


Figure 4 illustrates the boundary of CRA Downtown Orlando.

The proposed standards aim to guide redevelopment in a way that will create a community where people can live, work, and play. This involves enabling neighborhood-oriented uses to serve surrounding residents, and encouraging a mix of uses to increase positive activity throughout the day and early evening. Standards requiring improved sidewalks lined with street trees will improve safety and result in a more comfortable pedestrian experience, enhancing walkability throughout the community. The recommendations are intended to improve the quality of life of residents, while supporting the economic vitality of the corridor.

ORANGE BLOSSOM TRAIL CRA

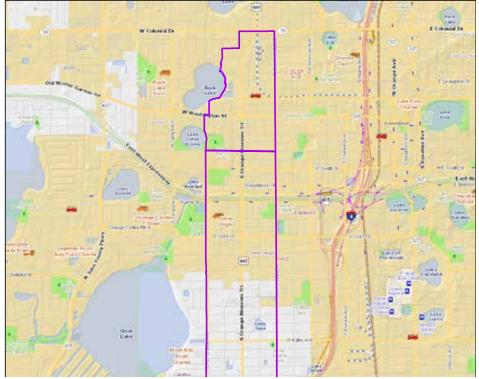
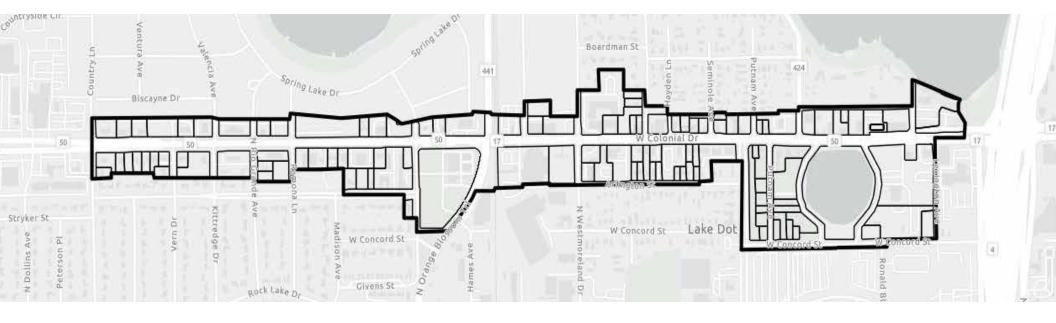


Figure 5 illustrates the boundary of Orange Blossom Trail CRA.



FINAL RECOMMENDATIONS

Summary

The intention of this study is to build on the momentum of Downtown and Creative Village to create an environment that encourages private investment along a corridor that is currently struggling with vacancy and visible deterioration of some structures. West Colonial Drive is an important gateway into Downtown as well as to Parramore and College Park. This gateway transitions from the largely suburban nature of Colonial Drive west of Tampa Avenue, to the urban, pedestrian-oriented character of Downtown and its adjoining neighborhoods. A well-performing gateway enhances the value of the City and the downtown.

Addressing the challenges identified through site visits and observations, data analysis, and community outreach will require a multifaceted approach. Identified needs include improved pedestrian and traffic safety, addressing public safety and homelessness, and attracting more desirable businesses along the corridor. The purpose of this study is to address land use and redevelopment regulations which can influence the outcomes of each of these areas. Solutions to these problems require cooperation that extends beyond the scope of this effort into other initiatives within the City or other agencies such as FDOT. Streetscape recommendations should be analyzed with the FDOT Design Manual, where design variances may be sought for variations.

Streetscape and site requirements should reflect a more human-scale environment, with frequent street trees, consistent and predictable streetscape widths, and buildings pulled closer to the sidewalks. The vision of this corridor reflects a more relaxed pace: small-format retail, neighborhood cafes and grocery, pocket parks, and more, all connected to the adjacent neighborhoods by a multimodal network that's safe for all users

By expanding neighborhood-oriented uses and prohibiting undesired uses, incorporating enhanced streetscape and urban design requirements, and activating underused spaces, redevelopment can occur in a manner that serves the community while contributing to the economic vitality of the corridor.

While this report includes specific streetscape recommendations for the corridor, traffic engineering improvements within the curb-tocurb area are currently under evaluation through the Orlando CROSS initiative (Connecting Residents on Safe Streets). This federally funded program was established to support the City of Orlando's Vision Zero objective of eliminating traffic-related fatalities and serious injuries by the year 2040. The study will identify optimal locations for targeted safety enhancements designed to improve conditions for all modes of transportation. See Appendix B for more information on this ongoing effort and prior planning projects addressing this study area.

Conclusions

In conclusion, these recommendations aim to address regulatory inconsistencies and unlock the full redevelopment potential of the corridor. By aligning zoning tools with the community's vision and enabling more flexible site planning, the City can better support housing diversity, economic vitality, and high-quality urban design.



Land Development Code Updates

Updates to the City's Land Development Code are recommended to address streetscaping requirements, permitted and prohibited uses, and buffering and screening requirements. The proposed standards would be implemented through a Special Plan Overlay District which provide specific design guidelines to achieve the goals and objectives of a specific area plan. This approach allows for enhanced standards beyond what is currently required by the Land Development Code within the West Colonial Drive Area and increased flexibility to encourage desirable investment. Below is a summary of the standards that should be considered for the overlay. A map of existing zoning can be found in the Land Use Analysis Chapter, Figure 19.

Use Regulations

Permitted uses vary between zoning districts within the corridor. In an effort to unify the corridor, a wider range of uses that can fill the needs of the neighborhood should be allowed in addition to any permitted uses within the applicable zoning district. Along with comments on uses throughout the corridor, community members express a desire for the west side of the corridor to have extended hours of usage. The predominantly office-oriented nature of the west side only sees activity on weekdays during regular working hours. A greater diversity of uses would keep activity constant throughout the evenings and weekends, allowing for a more vibrant corridor, and more eyes on the street. The uses listed in Table 1 align with common requests from the community stakeholders at the public outreach meetings.

Community meetings also included further discussion regarding food trucks and the City's current mobile vending policy as it applies within the study area. The City of Orlando recently adopted standards for mobile vending in 2024. Mobile vending can be accommodated as a temporary or permanent use throughout the study area, per Chapter 58, Part 4C.

Recommendations to Limit or Prohibit Uses

There are land uses which are not recommended within the overlay, regardless of zoning district, in addition to the existing prohibited uses. These uses were requested to be prohibited by community stakeholders or are incompatible with the desired uses and activities for the area

Use Type	Example Uses
Personal Services	Nail and hair salons, pet grooming, personal fitness centers (yoga, CrossFit)
Light Retail*	Grocery stores, drugstores, consignment shops, florists, retail sales up to 10,000 sq. ft.
Restaurants without drive-throughs*	Coffee shops, full-service dining, restaurant as defined by Sec. 58.705

Table 1 gives proposed additional uses permitted within the O-1 and O-2 properties within the corridor.

*In O-1/T and O-2/T properties, retail and restaurant uses should be limited to 3,000 sq. ft. or less.

Additional Recommended Prohibited Uses:

- Any business commonly known as a "check cashing" establishment, or any business in which a material part of its service includes offering loans secured by future employment wages or other compensation (often known as "payday loans," or "pay day advances"), but not including retail businesses which provide a check cashing service as an incidental part of their business and financial institutions such as banks, credit unions, and trust companies.
- Any business in which a material part of its service includes loaning money secured by vehicle titles (often known as "car-title loans"), but not including financial institutions such as banks, credit unions, trust companies, consumer finance, and retail installment lenders.
- Automotive services:
 - Automobile fuel, oil, and accessory sales.
 - Automobile body work.
 - Automobile detailing.
 - Automobile frame repair.
 - Automobile interior repair.
 - Automobile mechanical repair.
 - Automobile maintenance.
 - Automobile painting.
 - Automobile tune-ups.
 - Automobile tire sales and service.
 - Car wash.
 - Window tinting.
- Bail bond agencies, as defined by Chapter 648, Florida Statutes.
- Fortune tellers, tarot card readers, palm readers, psychics, and like establishments.
- Intensive Retail:
 - Open air markets.

- Utility trailer rental.
- · Vehicle sales and rental.
- Labor pools and labor halls as defined by Chapter 448, Florida Statutes.
- Liquor stores.
- Motels with individual units directly accessible from the parking lot and from exterior corridors with stairwells directly accessible from the parking lot.
- Neighborhood convenience stores.
- Nightclubs.
- Pawnshops, as defined by the Florida Pawnbroking Act.
- Principal use parking.
- Smoke shops.

Although the MU-1/T zoning district currently permits each of the uses listed above within the study area, community feedback has clearly indicated that these uses are not desired and should be explicitly prohibited.

The community expressed a preference for locally-owned businesses, which are less likely to be drive-throughs. Reviewing drive-throughs through the conditional use process allows the City to mitigate potential impacts and limit drive-throughs to appropriate sites.

Recommended Conditional Use: Drive-Throughs

- Limit drive-through businesses to avoid pedestrianconflicts and traffic circulation disruption.
- Position the drive-through and implement landscape screening to minimize traffic, noise, and visual impacts.
- Minimizing curb-cuts along West Colonial Drive to reduce pedestrian conflicts.

Community Concerns and Current Use Prohibitions

Feedback given from the community during the public meetings included concerns about uses that are already prohibited in the study area with existing regulations. These uses have already been deemed incompatible with the urban form of the area and the surrounding residential uses. These uses include:

- Adult entertainment venues: Currently only permitted within Industrial zones.
- Outdoor storage: Storage facilities not including passenger vehicles within 300 ft. of a residential district shall be completely within enclosed buildings, in accordance with Sec. 58.791.

Gun shop location restrictions are limited by F.S. 790.33 and cannot be explicitly prohibited within the study area.

Site Development Regulations

To promote a cohesive and visually appealing corridor, the Special Plan Overlay should establish consistent site regulations for all new development and redevelopment. Currently, the corridor includes multiple zoning districts with varying setback requirements, resulting in a fragmented urban form; buildings sit at inconsistent distances from the right-of-way, and some feature parking prominently in front. These inconsistencies disrupt the rhythm of the streetscape. The following recommendations aim to remove barriers to mixeduse development and foster a more unified and pedestrian-friendly environment by reducing setbacks and aligning parking strategies with the character of adjacent neighborhoods.

Sethacks

The current front yard setbacks for O-1 and O-2 are a minimum of 25 feet and 15 feet, respectively. Front yard setbacks for MXD-2 are a minimum of 20 feet. Proposed setbacks for the Special Plan Overlay along West Colonial Drive will mimic the MU-1 zoning district as they are the most permissive within the study area.

The proposed setbacks for West Colonial Drive are as follows:

- A 3-foot landscaping area from the inside edge of the sidewalk shall be built.
 - Landscaping should be provided in this area applicable to the landscape requirements of the Orlando Land Development Code. Benches, trash receptacles, and bike racks may be provided in this area. All building setbacks shall be measured from the edge of this zone.
- Front: 0 feet minimum to 15 feet maximum¹
- Side yard: 0 feet minimum to 25 feet maximum²
- Street side yard: 0 feet minimum to 15 feet maximum
- Rear yard: 20 feet minimum

¹ Where no other access points are feasible a larger front setback could be permitted by Transportation Official approval *if there is a driveway.*

² Where no other access points are feasible a larger side setback could be permitted by Zoning Official approval for a two way access point along West Colonial Drive.

The existing MU-1/T zoning district requires a 20-foot rear yard setback. This standard aligns with the front yard setback requirements of the adjacent residential zoning districts (R-2B/T and R-3B/T) located on the south side of the block between Westmoreland Drive and North Parramore Avenue, which front onto Arlington Street.

To promote compatibility between mixed-use and low-density residential uses within this corridor, it is recommended that where MU-1/T and residentially zoned properties are under common ownership within this block, the 20-foot rear yard setback be applied to the Arlington Street frontage. For architectural compatibility, it is recommended that the porch encroachment allowance within the Traditional City Design Standards is incorporated into the Special Plan Overlay for properties where the rear yard faces Arlington Street. This approach mirrors the front yard conditions of the adjacent residential properties, reinforcing a cohesive streetscape and supporting a more context-sensitive transition between zoning districts.

Encroachment and Design

The Traditional City Design Standards for Residential and Office Districts allow front porches with a minimum depth of 6 feet, and in some cases, second-story porches and balconies to encroach into the required front setback. These provisions encourage pedestrianfriendly design and architectural continuity along Arlington Street and should be maintained along with the additional standards of Section 62.600-62.605 for the residential and office districts within the corridor.

The Traditional City Design Standards for Mixed-Use and Activity Center districts (62.608-622.618) would continue to apply within the corridor with respect to the general standards and, when relevant, the use-specific standards.

Architectural Design

To strengthen the character of the community and support the long-term vitality of the corridor, architectural design guidelines should be established following additional community engagement. Working closely with residents, businesses, and stakeholders will ensure that the standards reflect the community's values and vision. Thoughtfully crafted guidelines can improve the aesthetic guality of development, foster a stronger sense of community and place, and help protect property values. Architectural standards should balance flexibility for developers with the community's desire for cohesive, high-quality design.

Landscaping and Bufferyards

During the community outreach session with business owners and developers, a recurring concern was the challenge of meeting all required landscape areas and buffers. The City's landscape code is



intentionally robust, promoting drought-tolerant species and Florida Friendly Landscaping™ practices, while aiming to enhance tree canopy coverage, support quality design, and align with impervious surface regulations across districts.

To support flexibility, the Land Development Code allows for a 50% reduction in required landscape areas, including bufferyards, through a Modification of Standards reviewed by the Zoning Official (Section 65.302). This tool helps balance landscape goals with the realities of site constraints.

That balance is especially important for smaller properties, where bufferyard requirements can be difficult to accommodate. For example, bufferyards required in the corridor range from 5 to 20 feet.

To encourage a more integrated mix of uses and expand the developable footprint within the study area, we recommend updated bufferyard requirements.

Within the Special Plan:

 Bufferyard requirements shall be exempt, except where properties directly abut one- or two-family residential uses.

Parking

Some of the feedback in the community outreach meetings included challenges in meeting the required parking on the smaller sites within the block bounded by Westmoreland Drive and Parramore Avenue. While the current code allows properties zoned R-3B/T to apply for additional parking spaces to support uses in buildings fronting West Colonial Drive through a Conditional Use Permit, this provision does not extend to the O-1/T and R-2B/T parcels on the same block. As a result, those parcels are excluded from utilizing this application type to satisfy parking requirements.

On the west side of the West Colonial Drive corridor, five blocks are zoned O-1/T. For properties with adjacent O-1/T zoning to the south, the Special Plan Overlay should permit Conditional Use Permit applications to allow additional parking to support uses fronting West Colonial Drive, provided the properties are under shared ownership or a binding lot agreement is in place. Although the parcels fronting West Colonial Drive in this area are larger than those within the block bounded by Westmoreland Drive and Parramore Avenue, the Special Plan Overlay aims to expand allowable uses within the O-1/T zoning district that may require more parking. For instance, while office uses require 2.5 parking spaces per 1,000 square feet of gross floor area, restaurant uses require 5 spaces per 1,000 square feet.

Together, these considerations highlight the need for a more flexible and inclusive parking strategy across zoning types within the corridor, ensuring that redevelopment potential is not constrained by inconsistent application of parking provisions protecting the same neighborhoods.

The Traditional City Design Standards currently require that parking be located to the side or rear for all development along this corridor.

The proposed Special Plan should require:

- Vehicular use areas to be screened in accordance with Sec. 62.611 throughout the corridor.
- No parking shall be permitted between the principal building and the street or within the required front yard or street side yard setbacks.
- Access shall be provided from side streets wherever possible to reduce the number of curbcuts along West Colonial Drive, including closing existing curbcuts where feasible.
- Within the Special Plan boundaries, R-2B/T and O-1/T may host additional required parking spaces for developments along West Colonial Drive through a Conditional Use Permit application, as is currently codified for R-3B/T zoning.
 - Additionally, in R-2B/T and O-1/T zoning districts, all parking must comply with the required front yard setback. The street wall and enhanced landscaping is required to remain consistent with the character of surrounding residential land uses.

Public Realm Improvements

Changes to a more consistent streetscape provides a more comfortable and predictable urban environment for all users, offering a visually appealing street and encouraging pedestrian activity. Wider sidewalks could accommodate pedestrians comfortably and people biking where bike lanes are not present. Shade trees offer respite from sweltering heat and beautify the neighborhood. Streetscape standards should be uniform between all uses and zoning, where possible.

Streetscape Requirements

During meetings with the community, concerns for traffic safety and perception of safety were brought up frequently, and site visits confirmed the lack of pedestrian and bicycle infrastructure necessary on a high-speed roadway connecting several neighborhoods. Some comments included a lack of lighting, shade trees, mid-block crossings, and bike infrastructure.

The lack of infrastructure in this area has discouraged residents and visitors from patronizing businesses along the corridor. Distances between crosswalks can be as long as 1,200 feet east of Orange Blossom Trail, and 2,500 feet west of Orange Blossom Trail. According to MetroPlan's Vision Zero, between 2018 and 2022, there were 28 crashes involving pedestrians or cyclists, including 2 deaths along this segment of West Colonial Drive.

Changes to the existing streetscape will be required upon any substantial improvement, enlargement, or redevelopment of a site. In order to establish a cohesive and safe streetscape application along this corridor, the recommendation is to measure the streetscape from back of curb. The streetscape diagrams following this page include the dimensional recommendations, meeting the performance treatment characteristics identified in the DTO Action Plan.

On-Street Parking

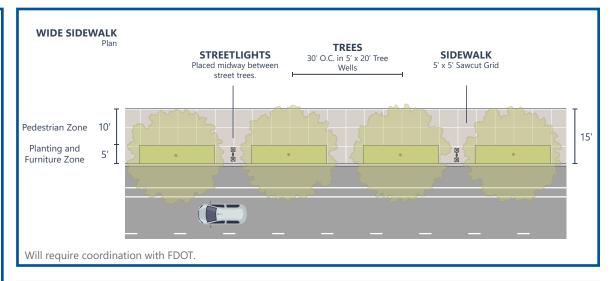
On-street parking is not required as part of the streetscape standards. However, if on-street parking is desired, the applicant shall provide a cross-section of the streetscape showing where the parking would be provided. Due to smaller sites on the east side of the corridor, the west side of the corridor is more appropriate to accommodate on-street parking.

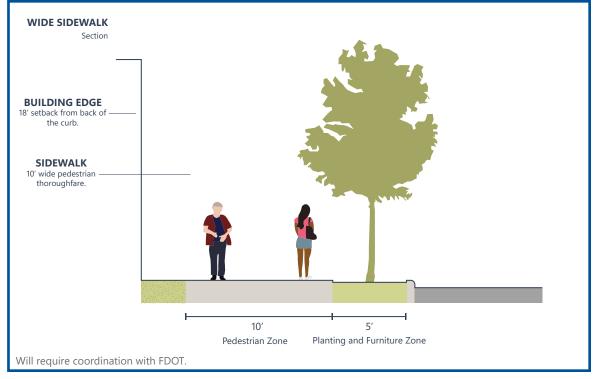
Parking along West Colonial Drive (if permitted) shall be designed to the following standards in the Special Plan:

- Parking shall not encroach the required 15-foot streetscape zone. Parking shall be designed in addition to the required 15-foot streetscape, but shall not encroach or impact the existing travel lanes.
- Parking shall not impede on existing bike lanes. Sufficient space must be provided for a 3-foot buffer between any existing or planned bike lane and a parking space per FDOT criteria. Provided that a 7-foot bike lane is provide (4 feet for travel and 3 feet of buffer) a parallel parking space may be a minimum of 7 feet in width.
- The parking lane shall be 8 feet wide, and parking stalls shall be a minimum of 20 feet long. A tree well area planted with a canopy tree shall be placed every 3 stalls.
- Existing canopy trees shall not be removed for on-street parking.

STREETSCAPE REQUIREMENTS FROM ORANGE BLOSSOM TRAIL EAST TO 1-4

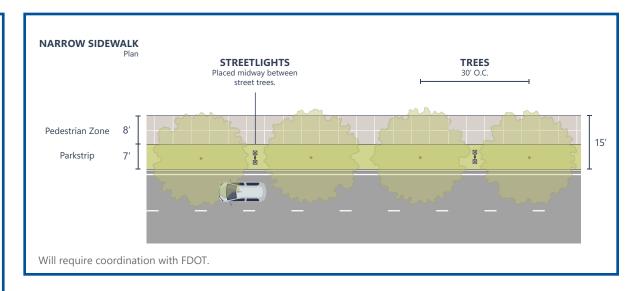
- Streetscapes shall be no less than 18 feet from the back-of-curb to the primary buildable area.
- The streetscape zone shall be provided as follows:
 - A 5-foot planting and furniture zone for canopy street trees shall be installed. Tree wells shall include soil cells and be a minimum of 10 feet in length. Tree wells may be lengthened up to 20 feet. Double acorn streetlights shall be placed midway between street trees. Space between the trees and acorn lighting should be concrete with a 5x5 sawcut grid.
 - Interior from the planting and furniture zone, a minimum 10-foot pedestrian zone shall be built and paved with concrete.
 - A 3-foot landscape strip is required between the back of sidewalk and the building facade.
- Between Orange Blossom Trail and Westmoreland Avenue, an additional 7 feet must be added to the streetscape from back of curb to accommodate the future extension of the bike lane, compliant with the required width for the FDOT's C4 classification.

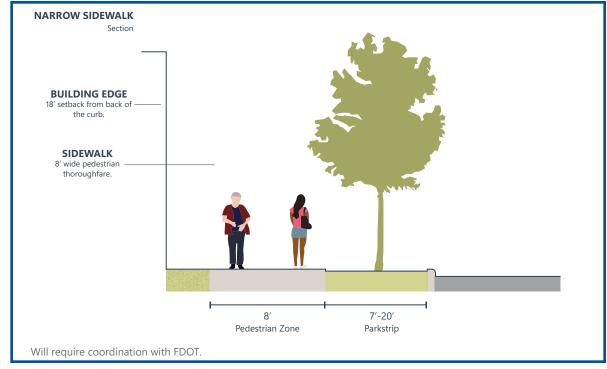




STREETSCAPE REQUIREMENTS FROM TAMPA AVENUE EAST TO ORANGE BLOSSOM TRAIL

- Streetscapes shall be no less than 18 feet from the back-of-curb to the primary building area.
- The streetscape zone shall be provided as follows:
 - A 7-foot parkstrip or tree well area containing canopy trees and grass shall be installed. Double acorn streetlights shall be placed midway between street trees.
 - An 8-foot pedestrian zone shall be built and paved with concrete.
 - A 3-foot landscape strip is required between the back of sidewalk and the building facade.
- Modification on the location of the streetscape zone may be requested based on context-specific exceptions, such as the existence of large canopy trees that are required to be conserved to be approved by the Appearance Review Officer.





Tree Preservation

Canopy Protection and Preservation Strategies

Corridor Specific Tree Protection Standards for Hardscape-Intensive Urhan Streets

To maintain and enhance the tree canopy along the primary streetscape within the study area, it is important to proactively address the inherent conflicts between existing mature trees and planned development. While current citywide tree protection standards apply, they do not account for the specific physical constraints introduced in the recommended street sections. Wider sidewalks, build-to-lines, and street wall provisions will often conflict with tree roots and crowns.

While well-intentioned, uniform policies can create unnecessary permitting friction, hindering efficient project delivery and potentially undermining corridor design objectives. Without tailored guidance, the Land Development Code provisions may conflict internally, complicating development processes.

To resolve these conflicts, commercial streetscape zones require streamlined, corridor-specific tree protection standards and preapproved protection measures designed explicitly for anticipated conditions. Such strategies must balance urban form, pedestrian accessibility, and tree health over time.

Specific suggestions for the Special Plan include:

- Section 60.211
 - Minimum Required Undisturbed Area: reduce the minimum radius from 0.75 feet per dbh inch to 0.5 feet per dbh inch.
 - Maximum Radius: reduce the radius from 24 feet to 20 feet

Specific dimensions and visual diagrams can be found in the Street Tree Analysis Chapter.

Consistent Tree Protection Standards for Residential Compatibility

Redevelopment pressure from commercial overlay districts may clash with the existing single-family residential tree canopy, potentially causing unintended tree loss, eroding neighborhood identity, and inciting community resistance.

To prevent these issues, enforce existing tree protection standards rigorously on residential streets such as Arlington Street and W Concord Street and the north-south streets that act as gateways to the residential areas. No new regulations are necessary; instead, consistent adherence to current measures will safequard tree canopy continuity and neighborhood character during periods of redevelopment.

By maintaining these protections, the residential character on the secondary streets can maintain a stable and resilient tree canopy that sustains environmental services such as shade and stormwater management and continues to reinforce the neighborhood character.

Strategic Flexibility to Support Urban Form and Development

Streamlined Mitigation Requirements for Front-Lot Tree Removals within Primary Streetscape

Balancing tree preservation objectives with urban design goals is important to support the development of a consistent, pedestrianoriented street wall, as recommended in this report.

Current regulations impose uniform tree mitigation standards regardless of a tree's location or the site's design intent. As a result, the permitting process for removing smaller-diameter trees within the front setback can create avoidable review hurdles. Current tree preservation provisions do not consider that urban form design parameters will inadvertently impact certain trees.

In the western half of the corridor, approximately 200 smaller trees conflict with the proposed site development requirements. In the same area, approximately 25 historic trees also exist, which conflict with the proposed site development and public realm requirements.

In the context of the commercial streetscape, the visual and functional benefits of a well-defined street wall may outweigh the ecological or aesthetic value of existing smaller-diameter trees, particularly those that may not have been managed in an urban-compatible manner.

The benefits of trees are more pronounced and better enjoyed as trees age and mature, so the same statement cannot be made for the larger and historic trees located along the corridor.

Specific suggestions for the Special Plan include:

 Revise mitigation standards to reduce or waive requirements for removing small trees (20 inches or less in diameter or in fair or worse condition) within proposed building footprints and within the front 100 feet for lots along West Colonial Drive.

Tailoring tree preservation policies to corridor-specific contexts will enhance development predictability and alignment with broader urban planning objectives.

Overhead Utility Treatment

Overhead utilities, excluding light pole service lines, are predominantly located on the south side of West Colonial Drive, from Westmoreland Drive east to Edgewater Drive, and from Lake Dot Circle east to just before I-4. These two segments account for approximately 25% of the commercial streetscape zone's frontage perimeter and will have both a functional and aesthetic impact on the eastern half of the corridor. Without utility modifications, large canopy trees will not be feasible in these sections.

If not properly planned, this constraint could result in long stretches of the corridor with reduced pedestrian shade and increased urban heat island effects, disrupting the continuity and comfort of the streetscape experience.

To mitigate utility-related constraints and enhance pedestrian comfort, adopt a two-part strategy.

Specific suggestions for the Special Plan include:

- Develop a limited palette of approved, utility-compatible tree species suitable for individual parcels where overhead lines remain.
- Provide financial or regulatory incentives to encourage underground utility relocation during block-scale redevelopment projects, promoting continuity and quality in streetscape treatments.

The Approved Plant List in Section 60.234 provides too many choices of overhead utility-compatible trees to support aesthetic continuity across these portions of the corridor, so a limited palette of up to four species is recommended.

Accelerating New Canopy Establishment and Performance

Urban Street Tree Species Selection Guide

To ensure the long-term health and compatibility of urban street trees, it is essential to select species that are resilient to climate conditions and compatible with surrounding infrastructure. Urban environments often subject trees to significant stress, leading to increased mortality, unsightly gaps in plantings, and a reduction in overall canopy coverage.

Beyond the overhead utility planting list discussed earlier, we also recommend the development and adoption of a pre-approved street tree species list tailored to the constrained conditions described in the proposed streetscape recommendations. This list should prioritize species with high drought tolerance, strong wind resistance, and sound structural characteristics, including a dominant central leader and well-spaced branch architecture. Market availability should also be factored in to ensure that species selection is practical and not overly aspirational.

Greater species variation should be promoted in nearby areas such as Lake Dot and adjacent residential streets to compensate for the reduced species diversity along the commercial corridor.

Specific suggestions for the Special Plan include alterations to the existing landscape code below:

- Section 60.288 Minimum Required Landscape Standards (MRLS)
 - Increase scoring multiplier for existing tree preservation.
 - Decrease or remove tree diversity scoring component.
 - Decrease or remove flowering tree scoring component.
- Section 60.216
 - Mandate annual reporting on tree establishment and conditions throughout the required three-year maintenance period.

Enhanced Tree Planting and Soil Quality Standards

It is important to require larger, structurally sound trees at the time of installation, supported by high-quality soil and ongoing oversight to accelerate canopy establishment and deliver early environmental and aesthetic benefits.

Under current regulations, relatively small-caliper trees are permitted, and soil standards are either minimal or unspecified. As a result, newly installed trees may offer limited initial shade and are more likely to develop poor structure after establishment if not structurally pruned. These issues can delay canopy growth and reduce the quality of shade and pedestrian comfort along the corridor.

To address this, we recommend requiring larger minimum-caliper trees at the time of planting along West Colonial Drive. These larger trees provide more immediate visual impact, establish strong structure early, and are less likely to have vertical clearance issues below the canopy.

Areas other than the primary streetscape may also benefit from the larger trees at installation, but the necessity of having larger trees is reduced compared to the West Colonial Drive public realm. Therefore, this recommendation is limited to the streetscape zone trees and not all required trees.

Specific suggestions for the Special Plan include:

 Require a minimum 4-inch caliper, 18-foot-high trees with an 8-foot clear trunk height along West Colonial Drive.

In addition to planting size, detailed soil volume and composition requirements should be prescribed or provided.

Specific suggestions for the Special Plan include:

 Specify soil volume and composition standards for uncompacted, biologically active soil within defined rootable areas. Providing approved soil types or sources would improve implementation.

Comprehensive Plan Updates

Subarea Policy - Density

To implement the proposed updates to the Land Development Code, a corresponding amendment to the City's Comprehensive Plan will be required.

Feedback from community outreach meetings indicates strong support for the introduction of mixed-use, multi-story development throughout the corridor. This feedback informed broader discussions around the need to increase allowable densities to accommodate a wider range and greater number of housing options.

Section 58.1101 of the Land Development Code outlines a robust density bonus program that, when specific criteria are met, can effectively double the number of residential units permitted on qualifying properties. Within the West Colonial Drive Corridor Study Area, this bonus is available to parcels zoned O-1/T, O-2/ T, MU-1/T, AC-2/T, and MXD-2/T. However, properties that are exclusively zoned for residential use are not currently eligible to participate in this program.

Drawing on the precedent set by the Alexan Mills 50 project in 2021, and the correlating Subarea Policy S.15.13, a new Subarea Policy should be established for the West Colonial Drive Corridor. The Subarea Policy should allow shared density across parcels under common ownership within the corridor. While this would not increase the total number of units permitted, it would remove the constraint of allocating a fixed number of units to each zoning district on a split-zoned site.

This provision would be particularly beneficial for the block bounded by Westmoreland Drive and Parramore Avenue, where the southern portion includes R-3B/T, R-2B/T, and O-1/T zoning. Under current regulations, only the portion of the site with zoning eligible under Section 58.1101 would qualify for a density bonus. However, allowing the total number of permitted units to be distributed across the

entire site would support more cohesive and efficient site design. This should not apply to intensity, where the R-3B/T and R-2B/T zoning districts have limited non-residential allowances.

Subarea Policy - Uses

The community outreach meetings also indicated support for a greater variety of neighborhood-serving uses in areas currently zoned O-1/T or O-2/T, particularly on the west side. The new Subarea Policy should expand the permitted uses allowed within the Office Low Intensity future land use designation which accounts for much of the western portion of the study area.

Subarea Policy – Social Services

The new Subarea Policy should establish goals to preserve the character of existing neighborhoods through buffer and transition requirements for properties adjacent to single-family. Additionally, to protect existing residential areas, the City should implement a Subarea Policy that contains similar language to Policy S.6.9 of the Future Land Use element, or extend the Policy to include the entirety of the study area. The referenced policy seeks to protect existing residential areas, encourage reinvestment, promote the fair distribution of social services throughout the region, and prohibits adult entertainment and new social services from being established in the Subarea.

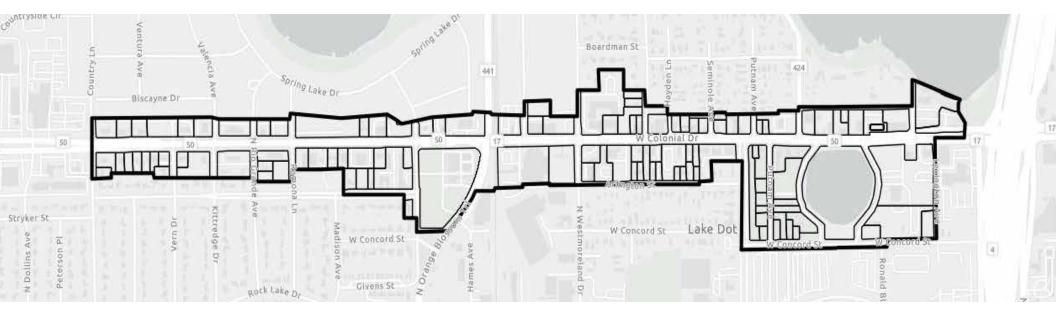
Non-Regulatory Strategies

While the vision for the West Colonial Drive Corridor will be implemented through updates to the Land Development Code and policies in the Comprehensive Plan, the City of Orlando also encourage investment through non-regulatory strategies, such as:

- Site activation through public-private partnerships to create plazas, art installations, and programming opportunities.
- Communicate the availability of grants for the Main Street Program and other business incentive opportunities.
- The community has expressed a desire for the City to provide a long term solution to the unhoused population in the area and limit social services uses. The City's ongoing efforts to address this concern include:
 - A subarea policy resulting from the Parramore Comprehensive Neighborhood Plan prevents new social services from being established within a portion of the study area.
 - City of Orlando Action Plan on Unsheltered Homelessness:
 - Goal to reduce the presence of unsheltered persons by 50% by 2027.
 - Provides pro-active, public space management, which is a services-first, diversion effort to coordinate housing and services for persons living outdoors.
 - Ensures no child spends the night on the streets of Orlando.
- Continue to maintain and enhance public facilities and amenities along the corridor including a focus on code enforcement and property maintenance.
- Provide more shade and lighting, with enough space to accommodate walking and biking and the potential for other gateway enhancements to improve the connection to downtown from Lake Dot, under I-4, and past Garland Avenue.



- Facilitate improved multimodal connectivity by removing physical barriers that limit access from northern neighborhoods to the corridor:
 - In the 1990s, the City—following community approval installed fences to close vehicular and pedestrian access to several streets north of Colonial Drive, between Orange Blossom Trail and Edgewater Drive.
 - The closures created dead ends to redirect traffic flow, encouraging use of Westmoreland Drive, Edgewater Drive, and Orange Blossom Trail for north-south travel.
 - One year later, Colonial Drive was expanded to five lanes, further separating the adjacent neighborhoods and reinforcing the traffic diversion strategy.
 - Without improved pedestrian connectivity, these physical limitations could significantly reduce local foot traffic and community engagement.
 - To ensure the viability of future commercial development, addressing walkability and neighborhood access should be a priority.



SAMPLE REDEVELOPMENT CONCEPTS

SAMPLE REDEVELOPMENT CONCEPTS

To help clarify the goals and vision of the Overlay, sample redevelopment concepts were created. These conceptual designs illustrate how new development, investment, and innovation could be achieved. These examples tested the land development code and comprehensive plan with proposed adjustments and showed the community what redevelopment potential could look like. These concepts would not be constructed by the City and are not limited to a particular site, but are intended to illustrate to property owners and potential investors creative strategies that could be implemented throughout the corridor.

Three sites were chosen for this exercise to show a range of locations, uses, zoning, and lot sizes. These sites were chosen in part based on an analysis of building to land value ratio that helps identify properties with the greatest redevelopment potential based on having relatively lower value buildings on high value property. An additional site was also chosen to give an example of a green infrastructure project that could serve as a shared stormwater facility for anticipated growth around the corridor.

Redevelopment Site 1: 1115 West Colonial Drive

This site, situated on the corner of West Colonial Drive and Orange Blossom Trail, provides a prime location and lot size for redevelopment.

Site Evaluation:

An 8-foot bufferyard is required containing a 6-foot-tall wall in the rear of the property that is adjacent to single-family residential as seen in Table 2.

Acres	Zoning	FAR	Setbacks	Height	Impervious Surface	Maximum Available Square feet
1.03	MU-1/T	0.50	0 minimum, 15 maximum	35 ft.	0.85	22,433

Table 2 explains the site metrics for Site 1: 1115 West Colonial Drive.

Redevelopment Concept:

The proposed use for illustrating redevelopment of this site is a food truck park. The site would have an L-shaped building along West Colonial Drive and the railroad tracks to ensure access from the east side on Ellwood Ave, and to provide interesting frontage for pedestrians walking along the sidewalk. Accessing the site from a side street prevents traffic congestion that could occur if access were allowed from Colonial. This use can activate the street and be a destination for the surrounding neighbors to eat, drink, and socialize.







Figure 2 depicts an aerial image of the redevelopment on Site 1: 1115 W Colonial Dr.



Figure 3 illustrates the proposed site plan for the redevelopment on Site 1: 1115 **West Colonial Drive.**

Redevelopment Site 2: 744 West Colonial Drive

This site is the former Burger King, and is situated on Colonial Drive with a rear that fronts on Arlington Street.

Site Evaluation:

An 8-foot bufferyard is required containing a 6-foot-tall wall in the sides towards the rear of the property that are adjacent to single-family residential as seen in Table 3.

Redevelopment Concept:

The proposed use of this site is a mixed-use development with affordable housing and ground-floor retail. The building is fronted on Colonial and is three floors, with ground floor retail and two stories of residential. Additionally, there is a residential-only building at the rear of the property facing Arlington. The site can be accessed by car from Colonial and Arlington, and parking is interior to the site, with tuck-under parking available for the rear building. This site offers smaller, neighborhood-serving businesses on the ground floor, attainable housing units 2-bedrooms and under, green space internal to the site, and compatibility with the neighborhood. Lowering the intensity of the use to just residential on Arlington allows for a softer transition for the surrounding single-family residences.



Acres	Zoning	FAR	Setbacks	Height	Impervious Surface	Maximum Available Square feet	Max du/ac	Parking
0.99	MU- 1/T/ PH	0.50	0 minimum, 15 maximum 20 ft rear	35 ft.	0.85	21,554	Affordable housing bonus: 49 Without bonus: 29	2.5 per 1,000 sf retail; 1 per studio unit (under 750 sf.)

Table 3 explains the site metrics for Site 2: 744 West Colonial Drive.



Figure 4 depicts an aerial image of the redevelopment on Site 2: 744 **West Colonial Drive.**



Figure 5 illustrates the proposed site plan for the redevelopment on Site 2: 744 **West Colonial Drive.**

Redevelopment Site 3: 709 Ramona Ln

This site is situated on the west side of the Overlay within an existing office park. This site was chosen as an example of what a mix of uses could look like in the primarily zoned O-1 area of the Overlay.

Site Evaluation:

An 8-foot bufferyard is required containing a 6-foot-tall wall in the rear of the property that is adjacent to single-family residential. as seen in Table 4.

Redevelopment Concept:

The proposed use of this site is to maintain ground-floor office use, with additional neighborhood-serving retail, and two stories of 3-to-4-bedroom condominiums. The building fronts on Colonial, allowing pedestrian access to the buildings. The property is accessed from Ramona Lane to the west and Colonial to the south, and parking is to the rear of the building. Tuck under parking is provided for the residential uses, and office parking spaces are available for guest and retail parking after hours. This example offers homeownership opportunities and housing for families, as well as activating the west side of the Overlay with active uses for more hours of the day.



Acres	Zoning	FAR	Setbacks	Height	Impervious Surface	Maximum available square feet	Max du/ac	Parking
0.99	MU- 1/T/ PH	0.50	0 minimum, 15 maximum 20 ft rear	35 ft.	0.85	21,554	Affordable housing bonus: 49 Without bonus: 29	2.5 per 1,000 sf retail; 1 per studio unit (under 750 sf.)

Table 4 explains the site metrics for Site 3: 709 Ramona Ln.



Figure 6 depicts an aerial image of the redevelopment on Site 3: 709 Ramona Ln.



Figure 7 illustrates the proposed site plan for the redevelopment on Site 3: Ramona Ln.

SAMPLE REDEVELOPMENT CONCEPTS

Green Infrastructure Concept

The City can investigate opportunities to improve water quality through stormwater management interventions on public property or through public-private partnerships. The study area falls within two local watershed basins, Shingle Creek Drainage Basin and Howell Branch Drainage Basin, and both have waterbodies directly impacted by West Colonial Drive stormwater runoff. These basins connect to larger watersheds such as the Kissimmee River Basin and Lake Okeechobee Watershed. The following lakes are impaired waterbodies on the Waters Not Attaining Standards (WNAS) list and are within half a mile of the study area:

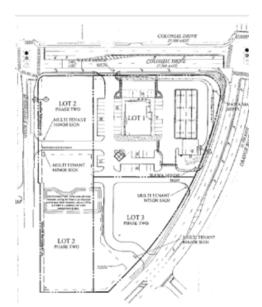
- Lake Dot is polluted by E. coli bacteria (directly in the study area boundary)
- Lake Adair and Spring Lake are over saturated with total phosphorus
- Lake Concord has a decline in biological community health
- Rock Lake is over saturated with total nitrogen

An excess of nutrients such as total nitrogen or total phosphorus can cause aquatic plants to grow too fast, choking water ways, potentially causing harmful algae blooms, and creating low oxygen conditions that pose a detriment to fish and other aquatic life. When a waterbody is listed as WNAS by the Orange County Environmental Protection Division (EPD) Water Sciences Section, a Total Maximum Daily Load (TMDL) is assigned. TMDLs limit the allowable pollutant levels and provide a plan under the Clean Water Act (Section 303(d)) to restore water quality.

One potential opportunity is the vacant land adjacent to the south end of the Wawa PD property, also known as Springdale Park. The City could consider a "Rain Garden" in this location to absorb and filter stormwater, enhance the park, and beautify the area. To increase feasibility, there is an opportunity for grants, awards, and water credits which can help to offset project costs and pave

way for infrastructure projects. The Veterans Memorial Park in Cape Canaveral, FL, is an apt case study for successful stormwater management project implementation. The project won the American Public Works Association (APWA) Florida Chapter Awards Program Project of the Year award for the less than \$2 million Building & Grounds project category. The honor was awarded due to the project's cost and time efficiency. The Veterans Memorial Park "Smart Rain Garden" received funding from the National Science Foundation's CIVIC Innovation Challenge. In addition to accolades and grant funding, the City is eligible for water offset credits, which can be purchased by authorized sources to meet water requirements, and in turn fund other infrastructure projects.

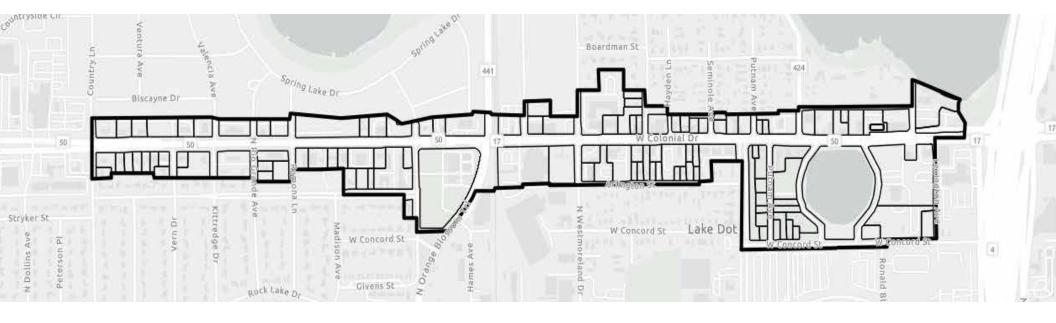
With the presence of impaired waterbodies in the immediate vicinity, increased stormwater mitigation and retention would complement efforts to maintain a high quality of life for the residents in the study area.







The site plan (above left) depicts the Springdale Park site which abuts the Wawa PD. The images (above right) depict the Veterans Memorial Park, a stormwater management project with a rain garden.



SAMPLE SHORT TERM STRATEGIES

Short-term creative placemaking strategies involve temporary, low-cost interventions that activate public spaces and foster community engagement. These can include pop-up parks, public art installations, street festivals, and tactical urbanism projects designed to quickly enhance the character, safety, and usability of a space. All these tend to create a sense of place that reflects the community's identity.

Site activations as a strategy for community/neighborhood revitalization and economic development can transform underutilized spaces into vibrant, community-focused areas that enhance the overall livability of communities. By encouraging collaboration between artists, residents, and local organizations, these strategies promote cultural vibrancy, strengthen social connections, and help inform longer-term improvements to the area. Ultimately, these strategies foster a more inclusive urban environment, promoting healthier, more sustainable communities.

The following short-term strategies are intended to illustrate how the City, the community, and property owners could help to build momentum and attract investment to the Corridor through temporary site activations. These short-term installations could demonstrate demand for business activity and engage neighbors in building a stronger sense of community. These visioning illustrations are intended to provide inspiration for the City and the community and do not represent installations that are planned to move forward at this time. The project team selected two sites to test sample creative placemaking activation scenarios based on the criteria outlined in Appendix C. One site has been identified for a pop-up plaza activation, and the other one to host a seasonal community park activation. Both site activations are recommended to be deployed during the dry winter season, spanning between January and March/April.

The short-term site activation is intended to run for 2-3 weeks or a month, while the seasonal activation is intended to run for more approximately 3-4 months. The following two locations have been selected for sample creative placemaking site activations:

- Arlington Street: Arlington Street Plaza (pop-up plaza)
- 911 West Colonial Drive: West Colonial Park (community park)

Both sites ranked high in terms of available parking, non-motorist access, main street partnership opportunities, site readiness, available shade, access to utilities, and MOT implications. They are both located in areas of high impact with high visibility for people traveling along West Colonial Drive and therefore have the potential to attract a large number of people. Both sites are located close enough to West Colonial Drive, however there's room to manage the proximity to S.R. 50 (West Colonial Drive), which has posted speeds of 40 mph, with buffers that allow for safe access and ability to create a comfortable environment for families and children. The sample short term strategies were developed prior to the release of the USDOT memorandum on July 1, 2025 and components such as intersection murals may not be compliant with FDOT policy resulting from the federally issued memorandum.

Arlington Street Plaza

Address	Arlington Street between Westmoreland Drive and train tracks.
Ownership	Public (City of Orlando)
Parking	McNamara Site, at the coin laundry across Westmoreland Drive, or at Office Solutions and McRae Art Studios.
Non-Motorist Access	Bus stop served by lines 48 & 49 less than 500 ft. away. Continuous sidewalks connect to the neighborhood. Shared-use path along Westmoreland Drive and bike lanes along Colonial Drive.
Main Street Partnership	Parramore Main Street Program - Zone 2
Site Readiness	Paved dead-end street.
Shade	Little shade, only small amount from adjacent building.
Access to Utilities	Street lights or adjacent private properties.
MOT Implications	Arlington Street will need to be closed.

Table 6 gives a brief site overview for Arlington Street Plaza.

Arlington Street Plaza is a dynamic site activation and placemaking strategy that re-imagines a segment of Arlington Street between North Westmoreland Drive and the train tracks (dead-end street). Arlington Street is located one block south of West Colonial Drive. This pavement to plaza activation has the potential of blending art, community, and play into a vibrant, multi-sensory experience. Divided into three interconnected zones, The Hall, The Fun, and The Lawn, this plaza activation comes alive with color, creativity, and energy, inviting people of all ages and abilities to celebrate, play and connect.

Concept Plan:

The Site Plan below lays out a concept for the Arlington Street Plaza site activation project. The concept consists of restricting/closing Arlington Street (the segment between the North Westmoreland Drive intersection and the train tracks) to vehicular traffic with the intent to activate the space as a pop-up plaza with community activities and events. Vehicular access to the Office Furniture Store (WMG Arlington Orlando Owner LL) will be maintained through the existing gate located at the dead-end of Arlington Street and through the Sign Depot's (VIP Investment, LLC) surface parking lot. During the week daytime hours, when street activation events are not taking place, vehicular access will be provided, as shown on the concept plan below. The Arlington Street Plaza concept plan illustrates the intent behind the site activation by proposing the placement and layout of key zones, furnishings, elements, and programming throughout the site. Key goals of this site activation include:

- Bring vitality to this community by offering them an inviting gathering space that strengthens the area's identity.
- Provide a space with activities for people of all ages and abilities for the community to connect and enjoy.
- Support local organizations, businesses and vendors as well as foster new economic opportunities in the area.

The Arlington Street Plaza is designed as a temporary site activation, intended to run between two to four weeks. The duration of the project will depend on factors such as maintenance of traffic (MOT) implications, buy-in and support from adjacent private stakeholders, available funding, and the ability to build the necessary partnerships to operate, maintain and program the space for the duration of the project. The concept plan also identifies the opportunity to implement two intersection murals (quick build projects) at the

intersections of West Colonial Drive & North Westmoreland Drive and Arlington Street & North Westmoreland Drive. High-visibility crosswalks are recommended to be installed at the time the intersection murals are implemented. It's recommended that high-visibility crosswalks are installed for all approaches at the intersection of Arlington Street & North Westmoreland Drive to enhance accessibility to the Arlington Street Plaza activation.







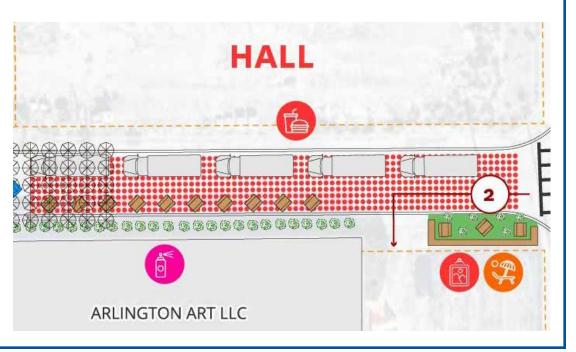
Figure 9 illustrates the site plan and layout of the different programs in Arlington Street Plaza.

*The sample short term strategies were developed prior to the release of the USDOT memorandum on July 1, 2025 and components such as intersection murals may not be compliant with FDOT policy resulting from the federally issued memorandum.

The Hall

The Hall zone is a lively social hub where community, art, and food intersect. Lined with food trucks, the space offers an array street food alternatives, providing guests with a culinary journey as they enjoy conversations over meals on nearby picnic tables. Guests can also unwind in an outdoor lounge surrounded by potted plants and landscaping, creating a cozy, green enclave in the middle of the urban landscape. As guests move through the plaza, an outdoor art gallery showcases local artists, with rotating exhibits that keep the experience fresh and engaging. The vibrant art murals on the outside walls of the nearby art studio and the colorful pattern stenciled on the pavement surface transform this section of Arlington Street into a walkable canvas, bringing the spirit of the pop-up plaza to life.

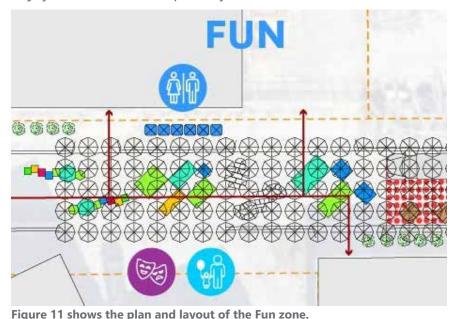
Figure 10 illustrates the floor plan and associated activities of the Hall.



The Fun

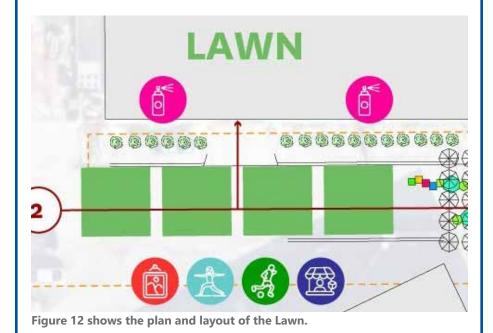
The heart of the activation, The Fun zone is designed for both play and entertainment. At its center is a flexible play zone for children, where asphalt art and painted game ideas such as hopscotch and a Candyland path create a fun backdrop to stimulate kids and foster a friendly outdoor playground environment.

The air is filled with laughter as kids run freely, playing in a colorful wonderland under the shade of a whimsical canopy of hanging colorful umbrellas. Adjacent to the play zone is a mobile stage that transforms this space into a versatile area, perfect for hosting live music acts featuring local artists, or even hosting a Super Bowl watch block party. Whether it's day or night, the Fun zone can be programmed with a variety of activities that bring excitement, offering a space to play and enjoyment under the open sky.



The Lawn

Lastly, the Lawn zone is a flexible open space where community wellness and culture thrive. With pieces of artificial turf covering the street asphalt, this zone becomes a hub for outdoor fitness, providing a space for groups to host yoga, jiu jitsu, and Zumba classes. This zone provides a space where residents and visitors can strengthen their health and wellness in the fresh air. The Lawn can also be a perfect spot to host during special event days a weekly farmers market or an arts and craft fair, enabling the community to come together and support local vendors. On non-event days the Lawn can be used for casual field games or as a lounge area that allows people to enjoy the open air. This zone also doubles as a cultural platform, with an outdoor gallery for art installations and murals on the exterior walls of the nearby warehouse, creating an engaging space for wellness, exchange and creativity.



West Colonial Park

Address	911 West Colonial Drive
Ownership	Private (Sun Beach Investment, Inc.)
Parking	Along Cordova Drive and adjacent commercial properties (limited parking areas)
Non-Motorist Access	Bus stop served by lines 48 & 49 less than 300 ft. away. Continuous sidewalks connect to the neighborhood. Shared-use path along Westmoreland Drive and bike lanes along Colonial Drive.
Main Street Partnership	Parramore Main Street Program - Zone 2
Site Readiness	Grassy, no built structure. Fenced. Accessible from Colonial Drive and Cordova Drive.
Shade	Plenty of shade trees.
Access to Utilities	Unknown.
MOT Implications	Cordova Drive will likely need to be closed (currently a dead-end).

Table 7 gives a brief site overview for West Colonial Park.

West Colonial Park is a dynamic site activation and placemaking strategy that re-imagines a vacant parcel adjacent to West Colonial Drive into a rich, multi-purpose community space where people of all ages can enjoy a mix of leisure, arts, and outdoor activities in a natural, welcoming park setting. Divided into distinct yet interconnected zones, the Soft Launch Garden, the Sneak Peek, the Bloom Court, and two Recharge Zones, this space comes alive with color, creativity, and energy.

Concept Plan:

The site plan below outlines a concept for the West Colonial Park site activation project. This project proposes transforming a vacant residential parcel adjacent to West Colonial Drive into a seasonal community park, hosting a variety of programmed activities and events. Pedestrian access will be available from both, West Colonial Drive and Cordova Drive, as indicated on the site plan. To enhance access to the park, the plan contemplates the implementation of a quick build intervention to provide temporary artistic crosswalk markings, providing a mid-block pedestrian crossing opportunity along West Colonial Drive. The existing concrete median will serve as a pedestrian refuge island. Additionally, two public transit stops, located within 300 feet of the site, are serviced by Lynx lines 48 and 49, which would also potentially benefit from the proposed midblock pedestrian crossing.





On-street parking is available along Cordova Drive, while offstreet parking can be accessed at two nearby parking lots, pending coordination with the property owners (College Park Place, LLC and Whitmire Properties, Inc.), as detailed in the site plan. The West Colonial Park concept plan illustrates the intent behind this site activation by proposing the placement and layout of key zones, furnishings, elements, and programming throughout the site.

Key goals of this site activation include:

- Bring vitality to this community by offering them an inviting civic space that strengthens the area's identity.
- Provide a space with programmed activities for people of all ages and abilities for the community to connect and enjoy.
- Support local organizations, businesses and vendors as well as foster new economic opportunities in the area.

The West Colonial Park is designed as a seasonal site activation, intended to run between three to four months, preferably during the dry and cooler months (between January and April). The duration of the project will depend on factors such as buy-in and support from private stakeholders, available funding, and the ability to build the necessary partnerships to operate, maintain and program the space for the duration of the project.

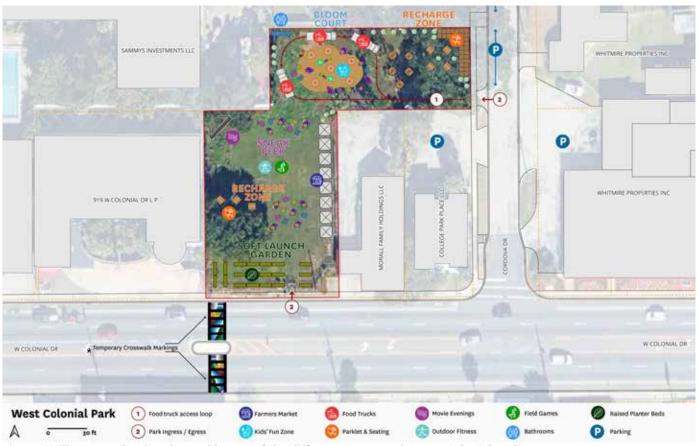
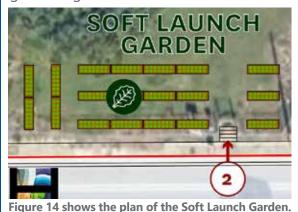


Figure 13 illustrates the site plan and layout of the different programs in West Colonial Park.

*The sample short term strategies were developed prior to the release of the USDOT memorandum on July 1, 2025 and components such as crosswalk murals may not be compliant with FDOT policy resulting from the federally issued memorandum.

The Soft Launch Garden

Placed along the West Colonial Drive frontage, the Soft Launch Garden provides a buffer between the roadway and the other park zones. The Soft Launch Garden features raised garden beds that will be ready to be filled with seasonal fruits and vegetables that are grown by and for the neighborhood residents. Neighbors are encouraged to collaborate in planting, tending, and harvesting fresh produce, fostering a sense of collective ownership and sustainability. A 6'x 6' container placed by the garden can provide storage space for essential gardening tools and materials for the park, ensuring that the garden remains well-maintained and functional. This zone provides an opportunity to not only grow food but also nurture relationships between residents in the area who choose to participate in the communal gardening activities.



The Sneak Peek

The Sneak Peek zone is an open grass area for recreational and cultural activities. Whether it's a pick-up ball game among friends, outdoor fitness and yoga classes, live music, or simply lounging with friends, this space offers flexibility for all kinds of gathering activities. A large screen can be mounted on a corner for outdoor movie screening events, where families can lay out blankets and enjoy films in the park. Along the eastern edge of the site, canopy pop-up tents can be lined to host farmers markets and arts and crafts events, providing a space for local artists and vendors to showcase and sell their products. The Sneak Peek is an energetic, ever-evolving space that brings the community together in both active play and creative expression.



Figure 15 shows the layout of the Sneak Peek zone.

The Bloom Court

Vibrant and lively, The Bloom Court is anchored around a kid-friendly zone, surrounded by three food trucks parked against the northern limits of the site, create a backdrop for this stimulating space. The play area, covered in a bed of mulch for safety, offers colorful teepees, painted tires, and various children's games, creating a joyful environment for kids to run, play, and socialize. Parents can keep eyes on the children in seating areas surrounding this space. Seating in this zone is made from re-purposed pallets and colorful tires, allowing them to stay close to their children while enjoying casual conversations or a quick bite from the food trucks. This zone blends fun, food, and familyfriendly activities, making it a hotspot for residents over the weekends.

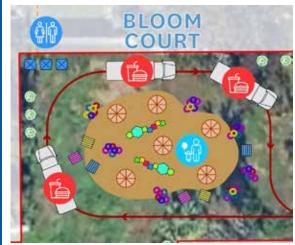
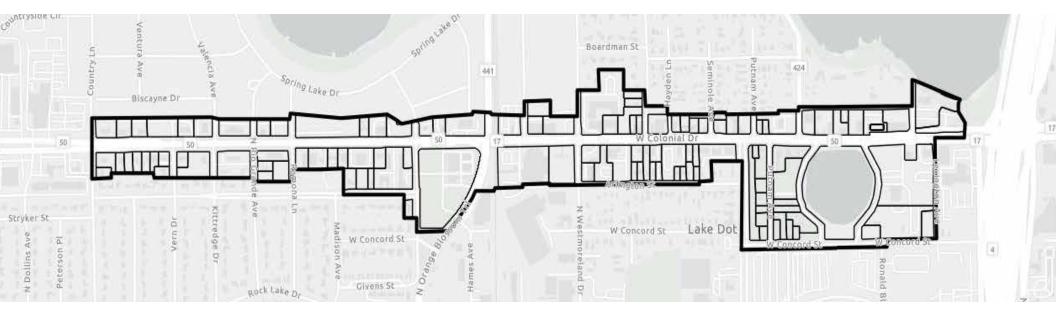


Figure 16 shows the amenities of the Bloom Court.



STRENGTHS, WEAKNESSES, OPPORTUNITIES, THREATS

A SWOT analysis of the West Colonial Drive Corridor was conducted in May of 2024 to establish a foundational understanding of the area's existing conditions, strengths, and vulnerabilities across three distinct segments: Traditional Office, Village Center, and Neighborhood Services District. This assessment identifies key physical and contextual assets such as mature tree canopies, human-scaled environments, and proximity to residential neighborhoods, while also highlighting critical challenges including high traffic volumes, fragmented pedestrian infrastructure, and underutilized parcels. The analysis serves as a strategic baseline to guide future planning efforts, emphasizing opportunities for improved connectivity, safety, and corridor identity.

Entire Corridor

From North Tampa Avenue to I-4

Strengths:

 High visibility with high passerby counts for business support.

Weaknesses/Threats:

- West Colonial Drive serves as a barrier with high volumes and speeds (posted 40mph) across a neighborhood commercial area.
- Lack of on-street parking to serve retail/service uses.
- FDOT control of roadway.

Opportunities:

- Improve Lynx stops options to make the public transportation option more attractive and functional for users.
- Gateway into Downtown Orlando from the north and west.

Traditional Office

From North Tampa Avenue to Springdale Road (2,600 feet) *Strengths:*

- Existing mature tree canopy.
- Continuous green buffer between sidewalks and roadway ranging from wide to narrow.
- Human-scaled environment.
- Well-kept and adapted traditional built form with most parking behind or beside buildings.
- High occupancy rates.
- Adjacency to residential neighborhoods.

Weaknesses/Threats:

- Large setbacks separating sidewalks from some building entrances.
- Occasionally missing street trees.
- Potential for loss of trees to redevelopment.

Opportunities:

- Allow neighborhood retail uses to service the surrounding neighborhood.
- Add/maintain street trees to beautify the corridor, improve the walking experience, and reduce vehicle speeds.
- Provide bike/pedestrian crossings across West Colonial Drive to reduce neighborhood fragmentation (Existing distance between available crossings is 2,575 ft.)
- Introduce traffic calming measures to fit traffic speeds with commercial character.

Village Center

From Springdale Road to Westmoreland Drive (1,300 feet) *Strengths*:

- High visibility and traffic volume provide opportunities for businesses and services to thrive.
- Newer investment on Wawa property including new streetscape and tree canopy.
- Public park at "Springdale Green."
- ArtReach Orlando facility.
- Some buildings directly adjacent to the sidewalk.

Weaknesses/Threats:

- Expansive intersection size. Not pedestrian scaled.
- Automobile-oriented design (no sidewalks on North Orange Blossom Trail, north of West Colonial Drive).
- Perception of lack of safety.
- Narrow sidewalks along West Colonial Drive.
- Lack of street trees east of North Orange Blossom Trail, along West Colonial Drive.
- Lack of border between sidewalk and roadway east of North Orange Blossom Trail, along West Colonial Drive
- Odd parcel configurations.
- Railroad tracks chop up parcels and act as a barrier.
- Poor or compromised residential street connections.
- Adjacent neighborhoods may face socioeconomic challenges that could affect perceptions of safety and desirability for new investment.

- Economic disparities in the area may limit the types of businesses and services that can be sustainably supported.
- Rising construction costs may deter potential developers and slow progress on large-scale projects.
- Redevelopment may lead to displacement of existing residents and businesses if not managed carefully, leading to social and community push-back.

Opportunities:

- Major crossroads are an opportunity to make a statement in the right-of-way (downtown gateway)
- Major vacant or under-developed lots ripe for redevelopment (remainder of Wawa PD, lot in the SW corner of West Colonial Drive and Westmoreland Drive).
- Create more pedestrian-friendly areas, including parks, plazas, and greenways, to improve livability and attract foot traffic.
- Leverage nearby cultural and institutions and Main Street programs to host events, markets, and festivals can draw visitors and build community engagement.
- Create integrate art installations and murals that celebrate the area's history and diversity.
- Incorporate educational public spaces for green infrastructure, such as rain gardens and bioswales and amenitized dry ponds.

Neighborhood Services District

From Westmoreland Drive to I-4 (3,500 feet) *Strengths:*

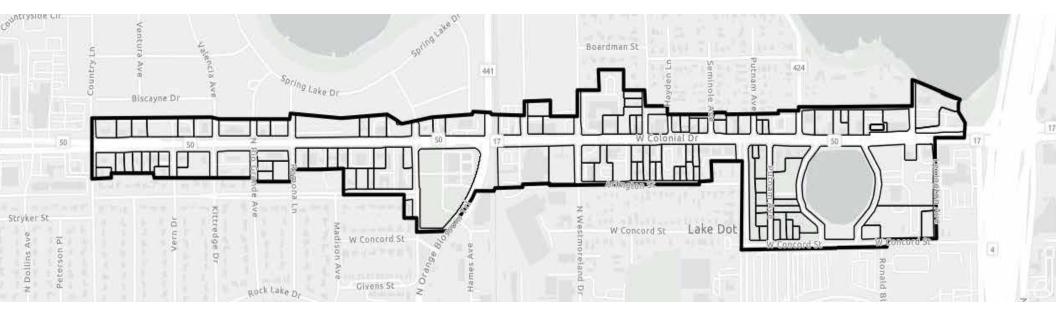
- Mixed-use potential with existing commercial and residential areas that can be further developed.
- Proximity/connection to UCF Creative Village.
- Existing built form conducive to Main-Street-style retail.
- Lake Dot public park.
- Recently redeveloped affordable housing units.

Weaknesses/Threats:

- Visibly vacant and rundown buildings, particularly retail.
- Narrow sidewalks about five feet wide in close proximity to the roadway.
- Lack of tree canopy along West Colonial Drive.
- Lack of unified theme or identity, making it less recognizable and less appealing as a destination.
- Certain existing businesses may not align with the character or needs of the surrounding neighborhood.
- Potentially illegal activities and business-based sales of items deleterious to human health such as smoke shops raise community concerns.
- Poor and compromised neighborhood transitions.
- Poor connectivity: walls or fencing barriers blocking vehicular and pedestrian connectivity on public streets north of West Colonial Drive on Hayden Lane, Seminole Avenue, Putnam Avenue.
- Perception of a lack of safety.
- Presence of social services increases presence of visible homelessness.

Opportunities:

- Redevelop vacant/underutilized parcels.
- Enhance existing bicycle lanes.
- Enhance connection to shared-use path along Westmoreland Drive.
- Provide bike/pedestrian crossings across West Colonial Drive to reduce neighborhood fragmentation.
- Create a vibrant mixed-use corridor that integrates residential, commercial, and recreational spaces.
- Create more pedestrian-friendly areas, including parks, plazas, and greenways, to improve livability and Mimic the new 10-foot-wide sidewalk installed on the north side of West Colonial Drive from Peachtree Road to I-4 westbound off-ramp.
- Repurpose 1 or 2 lanes of Parramore Avenue (south of West Colonial Drive) for community needs including active stormwater management, bike/ped improvements or other beautification efforts, since not all 4 are required to accommodate the traffic
- In tandem with the upcoming project "the Canopy" coming to I-4 additional programming should be studied for the intersection of West Colonial Drive and I-4. The intersection is a huge impediment to connectivity of the east and west sides of Colonial Drive. While the intersection was updated as part of the I-4 ultimate project, it is not a pedestrian or bicycle friendly intersection.



COMMUNITY OUTREACH

Executive Summary

Three business and commercial property owner outreach meetings and two general outreach meeting were held February through July of 2025 to provide an opportunity for the community to learn about and give feedback on the ongoing study of the West Colonial area. The objectives of the public engagement were to:

- Introduce the study and its results to date
- Document community member experiences of existing conditions including daily activities, quality of life, and challenges
- Listen to the community's desires for the future of the corridor
- Address guestions, concerns, and feedback about the study Overall public participation statistics:
 - Business Outreach: 60 attendees between three meetings (1 virtual and 2 in-person)
 - Community Meeting: 36 attendees
 - Final Community Feedback Meeting: 21 attendees

Public Outreach Efforts and Participation

Business and Property Owners Meetings

The three Business and Property Owners meetings were held on February 18 and 19, 2025.

To encourage broad community engagement, an informational handout was distributed to members of the West Lakes District Main Street and Parramore Main Street organizations. These organizations are the Main Streets that overlap each side of the study area. Additionally, the handout was directly emailed to identified community leaders and key stakeholders. Invitations were also extended to City Commissioners representing the districts within the study area, Districts 3 and 5.



In total, approximately 60 participants attended the meetings, including representatives from the West Lakes and Parramore Main Streets, West Lakes Partnership, Orlando Housing Authority, Orlando Sentinel, as well as several residents from surrounding neighborhoods. District 3 Commissioner Robert Stuart was also in attendance and a representative of the office of District 5 Commissioner Shan Rose

Participants were given a presentation on the team's findings for the area, including:

- Existing conditions such as land uses, zoning, and site visit observations.
- Prior planning for the area.
- Existing standards related to neighborhood compatibility including bufferyard and height standards.



Community Residents Meeting

A community meeting was held as an open invitation to the nearby residents on April 14th at the OCPS Academic Center for Excellence.

Outreach efforts for the community meeting utilized a multi-channel approach to maximize engagement. Five key databases were used, covering the Parramore Main Street district, West Lakes Market Street district, businesses in the 32801 and 32805 zip codes, and contacts gathered from walk-around visits. A fifth, project-specific database, included two City Commissioners, five neighborhood association presidents, and contacts resulting from direct correspondence received from the public during the study to date.

To ensure broad awareness, email blasts were sent out on April 1st, 9th, 11th, and 14th. Additionally, direct engagement efforts included walk-around visits to businesses on April 3rd and 4th, during which 63 businesses were visited. Outreach materials were also left at 21 vacant business locations to extend visibility.

Through these combined efforts, strong attendance and meaningful participation in the community meeting were encouraged.

Approximately 36 people attended the meeting, including representatives from the nearby neighborhood associations: Rock Lake (including Spring Lake Manor), Lake Dot, Spring Lake, and College Park. Also in attendance were Orange County Soil and Water Conservation District 1 Commissioner Samuel Chambers and an aide for interim Orlando District 5 Commissioner Shan Rose.

Participants were given a presentation on the team's findings for the area, similar to the presentation shared during the business meetings. Attendees were also shown concepts for redevelopment opportunities within the corridor. Three sites were chosen based on their high redevelopment potential, and illustrations were created to render how new standards could transform each property. In addition to the redevelopment opportunities, short-term activation opportunities were presented. These activation opportunities offered short-term (2 weeks to a few months) transformations of underutilized areas. Both the redevelopment opportunities and the short-term activation opportunities are illustrated and explained in depth in under Tab 7 Short Term Strategies.

Engagement activity boards were placed around the room to allow residents to provide feedback. All boards provided opportunities to place written comments. The boards included the following information and questions:

- Aerial map with the study area boundary
- Zoning map
- Desired and Undesirable Uses:
 - Where do you go now for common activities? (groceries, haircut, entertainment, parks)
 - Which of these activities would you like to do closer to home?
 - What uses would you not like to see in the study area?
- Priorities: "What are your priorities for the future of the neighborhood?"





Results and Findings

Business and Property Owners Meetings

Between the three meetings, many responses were collected on a few topics concerning the current and future conditions of the area.

DESIRED USES

- Sit down restaurants
- Dog parks
- Market rate housing
- Grocery store

DESIRED INFRASTRUCTURE

- Bike infrastructure
- Mid-block crossings
- Improved lighting
- More shade trees

PRIORITIES

UNDESIRABLE USES

- Motels
- Gas stations
- Pawn shops
- Liquor stores and vape shops

BIGGEST ISSUES ALONG THE CORRIDOR

- Need more flexible land uses
- Smaller buffers
- Loitering
- Vacant lots
- Distance between marked crosswalks
- Safety

General Community Meeting

The community meeting in April had similar results. The community echoed many of the same concerns and opportunities as the business owners. In addition, they provide specific feedback on the requested topic area. Photographs of the boards can be viewed in Appendix A.

Where do you go now for common activities?

Responses included going to Edgewater Drive in College Park for services like shopping, restaurants and personal services. Others mentioned going to Winter Park, Mills50, the Packing District, and Downtown for food, socializing, and recreation.

Which of these activities would you like to do closer to home?

- Personal Services
 - Yoga
 - Salon/Barbershop
 - Nail salon
 - Gym
 - Dog groomers
- Retail
 - Boutique shopping
 - Drugstore
 - Grocery/Green grocer
 - Flower shop
- Restaurants
 - Coffee
 - Breweries/Sports bars
- Affordable housing and market rate housing, with some opposing affordability
- Financial services/Credit union micro-branch

- Childcare
- Businesses that are an extension of the college/ university in Parramore

What uses would you not like to see in the study area?

- Tattoo shops
- Liquor stores
- Automobile service stations
- Smoke shops / marijuana dispensaries
- Nightclubs
- Strip clubs
- Pawn shops
- Massage parlors
- Car sales
- Convenience stores
- Money services (check cashing)
- Gun shops
- Motels
- Homeless shelter / social services
- Drive-through uses (although some members of the public advocated for these to be permitted)

What uses would you like to see more of in the study area?

Some of the most mentioned priorities included putting power lines underground, increasing bicycle and pedestrian safety including more crosswalks, lighting, and more attractive businesses. Safety was also a common answer, with specific requests to require Crime Prevention Through Environmental Design (CPTED) principles. These built environment tactics are meant to reduce crime by altering the physical environment and influencing human behavior.

Other notable priorities include:

- Art and signage that creates a sense of place:
 - A gateway into downtown
- Increased streetscape zone, including street trees, larger sidewalks, and landscaping.
- Increased residential density, including townhomes and mixed-use multi-story buildings (including senior housing).

Zoning map board comments include:

- Allow higher density housing throughout
- Create policies to protect existing homes
- Create code to save huge trees
- Reduce non-residential uses into residential areas, especially areas fronting residential homes
- Square up zoning including adjacent to the corridor

In the aerial map board activity, participants were asked to place a numbered sticker to a spot on the map and write on an index card their comment on that point. These comments included sitespecific issues or opportunities, as well as area-wide comments. The comments are displayed with their sticker locations in Appendix A. The concerns given in these comments were the replacement of removed trees, areas where development should happen, and concerns over pedestrian safety.

Final Community Feedback Meeting

A fifth community meeting was held as an open invitation to the nearby residents on July 17th at the Heart of West Lakes Wellness Center at 6:00 p.m.

Outreach efforts for the community meeting were sent from a combination of City and consultant contact lists using information

gathered in preparation for the first community meeting. Email blasts were sent out on June 25th, July 8th, and July 14th.

Approximately 21 people attended the meeting, consisting mostly of residents and business owners that attended one of the previous four meetings, with an additional eight attendees who attended their first meeting. The audience included a mix of local business owners, community leaders, and residents from the surrounding neighborhoods like Spring Lake, Rock Lake (including Spring Lake Manor), and Lake Dot.

Participants were given an updated presentation on the team's recommendations based on findings from the research on the area and feedback from prior public meetings. The meeting contained a presentation on:

- Summary of existing conditions and prior planning presented at the previous meetings.
- Potential regulatory changes to the streetscape, permitted uses, site development regulations, and tree protection.
- Other opportunities based on public concerns, like enhanced code enforcement, public safety, and additional Lynx stop improvements.

There were a few key issues and recommendations voiced by the mix of local stakeholders:

Zoning and Regulatory Changes:

- Stakeholders expressed concerns about the challenges to implementation posed by state legislation (2025 SB 180), which restricts jurisdictions from putting in place more restrictive regulations.
- The need to coordinate with City departments to ensure awareness of the study consistent messaging to those interested in developing properties while the study is being implemented.

- Buffers should be maintained for single-family uses even when they are not on properties zoned only for single-family.
- Residents brought up concerns about maintaining a height transition from the corridor to the residential areas.

Tree Canopy and Landscaping:

 A resident emphasized the importance of having a consistent, high-quality tree canopy and landscaping along the entire corridor, not just in certain sections.

Redevelopment:

- Sentiment among the attendees included restoring a sense of pride on Arlington Street.
- A stakeholder expressed interest in architectural standards to require new construction be consistent with existing architecture in the area.

Parking and Access Management:

- A stakeholder raised concerns about ensuring appropriate parking lot design and access management, particularly in areas adjacent to residential neighborhoods, to minimize impacts.
- A stakeholder expressed concern that limiting drive-through uses could discourage some desirable businesses that would benefit from the high pass-by traffic.
- Shared parking was brought up as something the overlay should encourage.

Overall, the key issues centered around ensuring the proposed regulatory changes align with stakeholder expectations, preserving and enhancing the corridor's visual character, and managing the impacts of redevelopment on adjacent neighborhoods.

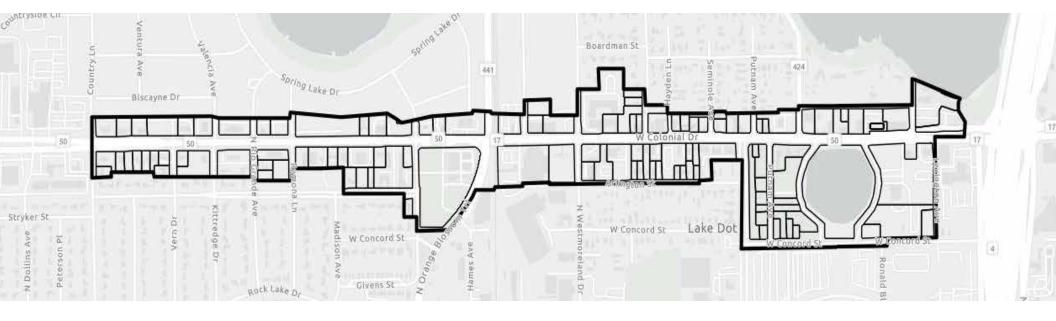
Conclusion

The meetings with business/property owners and community residents revealed strong alignment on the need for revitalization that prioritizes safety, walkability, and a mix of neighborhood-serving uses. Both groups expressed a clear preference for more sit-down restaurants, grocery options, boutique retail, and personal services, along with a strong opposition to uses seen as detrimental, such as motels, pawn shops, liquor stores, and smoke shops.

After the recommended changes were presented in the follow-up meeting, the alignment remained the same, with further emphasis on ensuring that residential uses are properly buffered from the more intensive uses of the corridor.

Infrastructure improvements are a major priority, especially enhanced pedestrian and bike safety through better lighting, midblock crossings, shade trees, and streetscape upgrades. Safety concerns, particularly loitering, vacant lots, and the larger population of unsheltered persons are top issues that need to be addressed through environmental design and policy changes.

There is also strong support for more flexible land use and increased residential density, while maintaining the character of existing homes and preserving mature trees. The community envisions a corridor that reflects its identity through attractive businesses, art, and thoughtful zoning, creating a more vibrant, inclusive, and livable neighborhood.



LAND USE ANALYSIS

Introduction

This section explores the existing land uses within the West Colonial Drive Study Area and proposes additional uses which could encourage investment in the corridor. The Study Area currently consists of office, commercial, and residential. Of the study area parcels, 32 are vacant, providing opportunities for new uses to activate the corridor. Other standards which work in tandem with permitted uses to create a thriving community are considered, acknowledging the interconnectedness of the streetscape, landscaping, and the use of a building.

Methodology

The data used in this analysis was retrieved from Orange County Property Appraiser on April 22, 2024, which provides Property Use Codes based on the most recent appraisal. The codes were grouped into relevant land use categories and mapped using ArcGIS.

Some properties required further analysis to determine the accurate land use. A visit to the study area revealed vacant buildings on properties listed as an active use by Property Appraiser. To find the most updated information on these buildings, real estate data was retrieved from CoStar. This data provided insight regarding the lease and tenancy of each property and the classification of buildings. Several of the properties with empty buildings have an active lease. Where CoStar provided more updated information than Orange County Property Appraiser, the land use classification was updated. The land use classifications in some cases were updated manually based on the observed conditions of the existing business. This information provided a deeper understanding of the corridor and the potential for redevelopment.

Existing Land Uses

A summary of acreage distribution by existing land use within the Study Area is shown in Figure 17. A map of existing land use is shown in Figure 18 on the following page. Overall, Office uses make up almost half of the uses in the corridor. The second most common occurrence is vacant land.

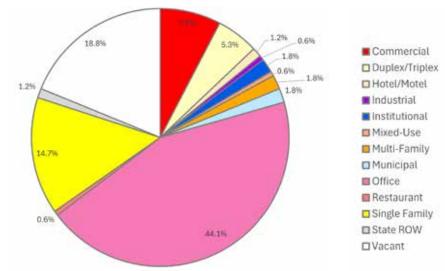
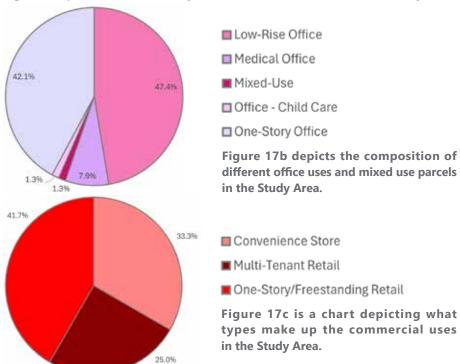


Figure 17a provides a visual analysis of the different land uses in the study area.



The section of the Study Area west of Orange Blossom Trail consists mostly of offices, with pockets of commercial. There is a range of residential uses in non-residential zones, including single-family, duplexes, and multifamily. This section of the corridor is where most of the vacant parcels are located.

The majority of offices within the corridor are low-rise offices (2-3 stories) or one-story offices, with a small percentage consisting of medical offices. These include uses such as law offices, property management and real estate, and dental offices. According to data retrieved from CoStar, office buildings along the corridor include Class B and C, which denote the general condition of the building. Although these buildings lack the amenities of a Class A building, there may still be a market for these spaces due to lower rents. Class C Buildings account for 80% of office buildings in the Study Area and are generally optimal for redevelopment due to their age and condition.

Commercial uses account for 7.6% of the acreage within the Study Area consists of one-story/freestanding retail, convenience stores, and multi-tenant retail. Based on the analysis of existing land uses, retail and services intended to serve the surrounding neighborhood are limited. While two convenience stores offering everyday supplies and two restaurants exist, there are many personal service, restaurant, and retail services that could better attract visitors and support the nearby communities including: restaurants or food truck

parks; retail establishments including a grocery store or regular weekly farmer's market to provide better access to local produce, thrift or vintage shops, florists, record shops, and outdoor gear or bicycle shops; and personal services such as veterinary clinics or pet day cares, hair and nail salons or barber shops, art studios, dance or fitness studios, tailoring, and repair shops for bikes, shoes, or electronics.

The vacant properties in the corridor consist of both vacant land, and parcels with an active lease which are being paid despite the vacant building. Vacant properties with an active lease being paid by a national retailer indicate a potential disconnect between the interests of property owners and the interests of nearby residents and businesses that could limit or slow redevelopment.

Proposed Land Uses

Expanding the permitted uses within the study area to allow a greater mix of uses and neighborhood-oriented services could encourage investment and redevelopment along the corridor. Proposed uses to activate the corridor and support the surrounding neighborhoods include personal services and eating and drinking establishments. Personal services are generally establishments to serve individuals, such as hair salons, spas, veterinary services, photography studios, or tailoring. These uses are currently permitted on the east side of the Study Area and should be expanded throughout the corridor.



Figure 18 is a map of existing land uses in the study area.

Personal services and eating and drinking establishments are not permitted in O-1 or O-2 but could benefit the community if permitted in these zones in the corridor. Additional standards to consider might include a cap on the total square footage per tenant and per lot to ensure these uses remain in appropriate neighborhood scale. For example, these additional uses could be limited to 2,000 square feet per tenant, and 8,000 square feet per lot.

Currently, the food services in the corridor are primarily fast food with one full-service restaurant. Small eating and drinking establishments such as a coffee shop or food truck park could

activate spaces during the day and into the evening, creating a community where residents and visitors alike can spend time.

Small businesses and dining opportunities will create job opportunities and attract more visitors, stimulating the local economy and establishing a unique community character. With neighborhood services within walking distance to the surrounding neighborhoods, the time and distance to travel to access goods and services will be reduced, contributing to quality of life. Permitting these uses within the corridor is critical to redevelopment efforts and fostering economic vitality.

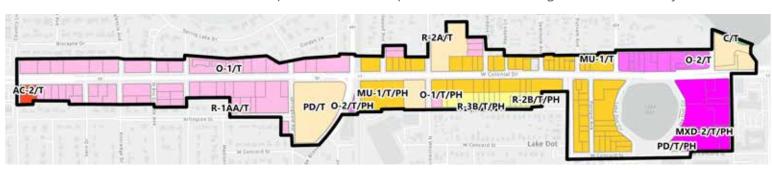


Figure 19 is a map of zoning in the study area.

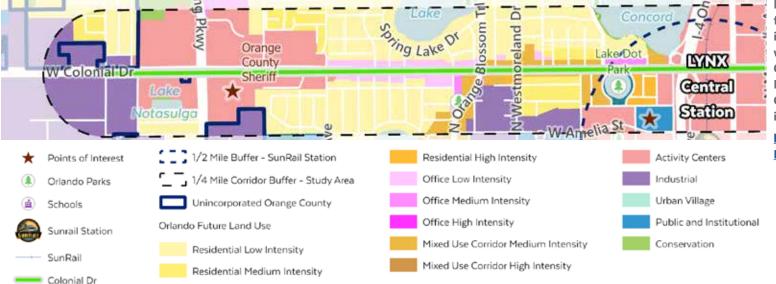


Figure 20 is a map illustrating future land use designations in the Study Area. This map was developed for the City of **Oralndo's Vision Zero: Connecting** Residents on Safe Streets (CROSS) Community Snapshot. More information can be found here:

https://www.orlando.gov/ Initiatives/Vision-Zero/CROSS With the intent to strengthen the variety and compatibility of the commercial land uses and redevelopment opportunities along the corridor, while enhancing pedestrian safety, certain uses which are currently permitted in the east portion of the Study Area as well as the single AC-2 zoned property at the western edge of the Study Area should be prohibited. Table 8 lists the uses are prohibited uses requested from public feedback for within the corridor.

Additional Considerations

Expanding the permitted uses within the Study Area cannot create the desired community character alone. Improvements to the streetscape are critical to shaping the social, economic, and environmental fabric of the community. Currently, the east side of the Study Area where the mixed-use zoning district is prevalent allows a range of the front setback from 0-15 feet. The west part of the Study Area in the office zoning district requires a 25-foot

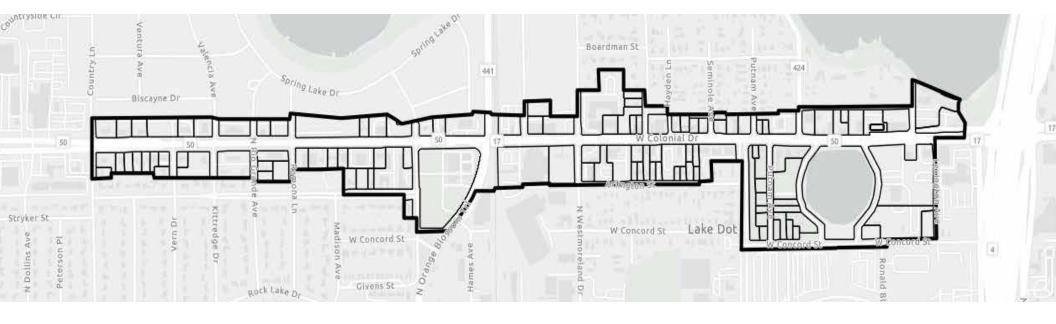
minimum setback with no maximum. Reducing setbacks to bring buildings closer to the street in the office district would create continuity along the corridor, and contribute to establishing an urban form. This newly defined public realm will include street trees to provide shade and wide sidewalks.

Enhancing walkability and connectivity can support local businesses by increasing foot traffic through the area and putting slower moving customer's close to businesses' front doors. An attractive streetscape increases the appeal of the community, leading to further investment and development.

Other potential elements to enhance the vitality of the community and contribute to the success of the uses in the corridor include wayfinding signs, measures to slow traffic, public art, and local events to engage residents and neighborhood groups in their community.

Use	Concern to address	Proposed Solution
Tattoo shops, liquor stores, pawn shops, car sales, motels, smoke shops, bail bonds, check cashing, gas stations, fortune tellers	Public comments to prohibit	Prohibit in overlay
Adult entertainment, nightclubs, outdoor storage	Public comments to prohibit	Already prohibited in this area
Gun sales	Public comments to prohibit	Pre-preempted by state law
Drive-through	Pedestrian Safety, Desire for more local businesses & sit-down restaurants.	Conditional use where currently permitted
Massage Parlors	Public comments to prohibit	Already screened through Orlando business permitting process to require licensed professionals
Social Services	Public comments to limit/ prohibit	Discuss with City leadership

Table 8 provides a list of uses which were requested to be prohibited by the public.



HEIGHT AND BUFFER REQUIREMENTS

Height Requirements

Figure 21 below depicts the maximum permitted heights within each zoning district within the Study Area boundary. The maximum permitted heights range from 30 feet to 100 feet. Florida's Live Local Act may effectuate increased height within the corridor, and is further explained in Appendix D.

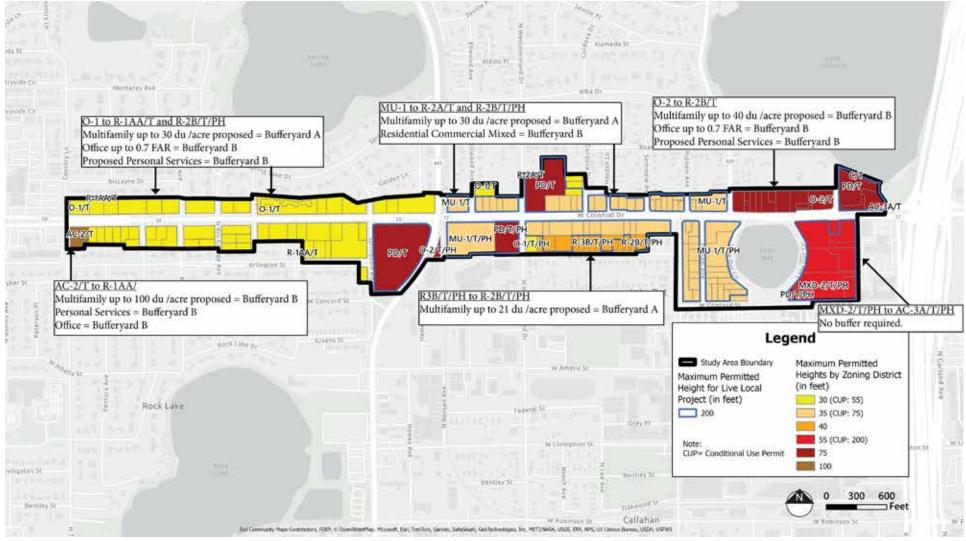


Figure 21 is a map of height restrictions in the Study Area.

Buffer Requirements

Orlando's Land Development Code establishes bufferyard requirements based on the intensities of adjacent uses as seen in Figure 21. The Study Area contains office and mixed-use zones with a range of densities and intensities adjacent to single-family residential. These zones permit multi-family residential, offices, and commercial. Of the zones within the study area, personal services and neighborhood scale eating and drinking establishments are currently only permitted within AC-2 and MU-1 but could add value to the O-1 and O-2 zoning districts along the western portion of the corridor.

Multi-family uses up to 21 du/acre and 30 du/acre, which apply to R-3B, O-1, and MU-1, require Bufferyard A when adjacent to existing single-family residential.

O-2 permits up to 40 du/acre and AC-2 permits up to 100 du/acre, both of which require Bufferyard B when adjacent to single-family residential. Offices, mixed-use, commercial, and personal services also require Bufferyard B.

More intense uses are permitted by right or conditionally within the MU-1 and AC-2 zones, such as nightclubs and drive-throughs. When adjacent to single-family residential and multi-family up to 30 du/ac, Bufferyard C is required. When adjacent to multi-family 30 du/ac and above, office, or commercial, Bufferyard B is required.

A nuance in the existing buffer requirements is that Class III and Class IV uses, as established by FIGURE 3. LAND USE INTENSITY TABLE from the Land Development Code, both require Bufferyard B when adjacent to single-family uses. A potential solution is to include a modified Bufferyard B, increased to a minimum width of 8 feet with an average of 12 feet to apply to Class IV uses adjacent to single-family uses. Another option would be to include a mandatory Bufferyard C for developments above three stories adjacent to single-family uses.

However, additional buffers would limit the development capacity of small, infill redevelopment sites, creating barriers to investment.

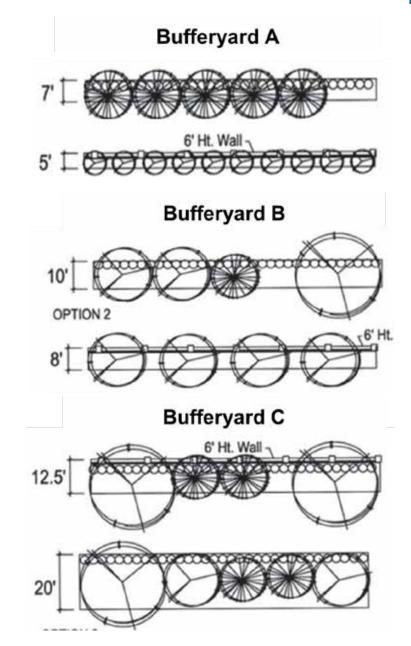


Figure 22 illustrates different bufferyard options for different scenarios. Compare to Table 9.

HEIGHT AND BUFFER REQUIREMENTS

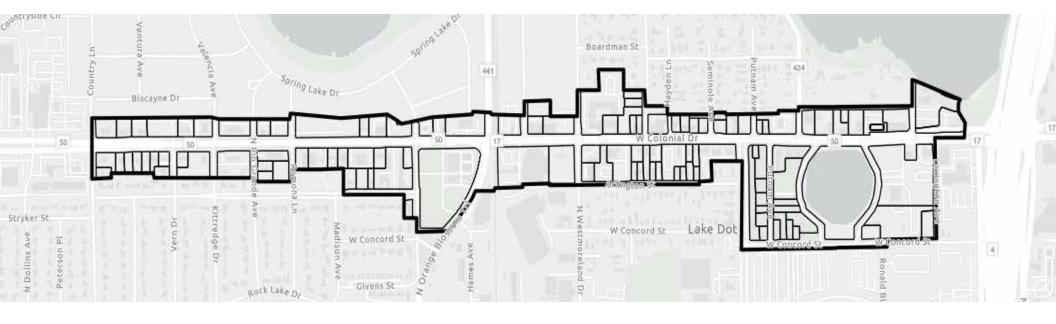
The existing buffer requirements should provides a sufficient transition between the proposed and encouraged uses within the Study Area and the surrounding residential neighborhoods. For these reasons, increases to the buffer requirements are not recommended at this time. Other standards can contribute to a transition between

uses of different intensities, such as setbacks. Smaller front setbacks will locate buildings closer to the frontage and away from the rear lot line abutting single family uses, minimizing the visual impact of the more intense use.

	Bufferyard A	Bufferyard B	Bufferyard C
Option 1	5 ft. Width	8 ft. Width	12.5 ft. Width
	6 ft. Tall Wall	6 ft. Tall Wall	6 ft. Tall Wall
	80% Tree Coverage	80% Tree Coverage	80% Tree Coverage
Option 2	7 ft. Width	10 ft. Width	20 ft. Width
	1 Row Evergreen Shrubs	1 Row Evergreen Shrubs	1 Row Evergreen Shrubs
	80% Tree Coverage	80% Tree Coverage	80% Tree Coverage
Relevant uses when adjacent to single family homes	Multifamily up to 30 du/acre or less	Offices Personal Services Mixed-Use Multifamily over 30 du/acre Restaurants Light Retail or Shopping Centers Principal Use Parking ¹ Hotel & Motel Commercial Dwelling Units	Night Clubs1 Drive-throughs Light Manufacturing & Processing Intensive Retailing

Table 9 provides a summary of existing buffer requirements in relation to applicable examples.

¹ This use is recommended to be prohibited for the Study Area due to the residential adjacencies, per the Land Use Analysis Chapter.



STREET TREE ANALYSIS

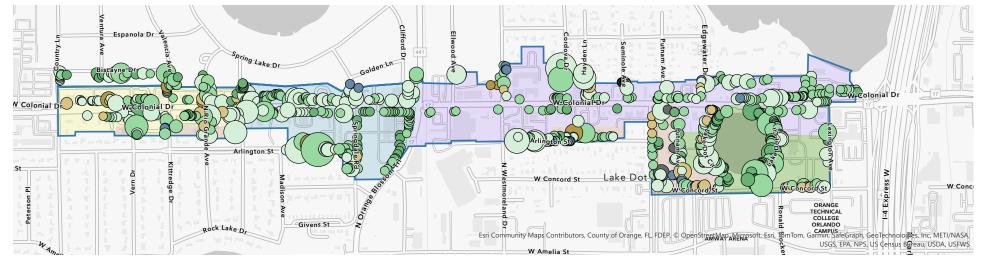
Street trees are a critical element to creating walkable communities, as they provide shade and beautification, as well as contribute to traffic calming measures to improve traffic safety. When the correct type of tree is selected, street trees also provide numerous environmental benefits such as stormwater management, reduction of the urban heat island effect, and supporting biodiversity for species such as birds and pollinators.

The Study Area has a tree canopy coverage of 25.8%, compared to the City of Orlando's 27% coverage. Tree conditions within the Study Area range from poor to excellent; as redevelopment takes place, healthy trees should be preserved, and trees in poor conditions should be removed.

Native trees are integral to Florida's ecosystems and are highly encouraged, as they support local biodiversity and contribute to ecological stability. Some exotic trees offer similar ecological benefits and add aesthetic value, but can disrupt ecosystems if the wrong species is selected or growth is left unchecked. Any non-native tree species that are planted should be Florida Friendly™. Invasive species should never be used, as they can kill existing local flora and fauna, spread aggressively, and result in biodiversity loss and alter ecosystem functions. Currently, 8% of trees within the Study Area are invasive. These trees should be removed upon redevelopment and replaced with a combination of native and Florida-Friendly™ species.

The results of this analysis inform recommendations and strategies for each section of the Study Area in order to best meet the community's needs, resulting in healthy and beautiful street trees which fit the character of each area.





Legend



Figure 23 illustrates the Landscape Zones of the study area, as well as tree condition.

Commercial West

14.8%

of All
Street Trees:

Trunk Area Contribution

Street Trees:

106

Tree Species:

18

Transitional Areas

18.8%	Street Trees:	135
of All Street Trees	Tree Species:	25
Trunk Area Co	ontribution	311 SF

Lake Dot

18.1%	Street Trees:	130
of All Street Trees	Tree Species:	17
Trunk Area Co	ontribution	328 SF

Commercial Center

19.2%	Street Trees:	138
of All Street Trees	Tree Species:	19
Trunk Area Co	ontribution	257 SF

Commercial East

29.0%	Street Trees:	208
of All Street Trees	Tree Species:	25
Trunk Area Co	ontribution	297 SF

Overall Canopy Coverage

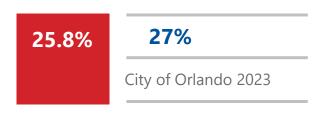


Figure 24 is and infographic depicting the distribution of trees across the Landscape Zones.



Figure 25 shows the different types of trees which are prevalent in the Study Area.

Tree Diversity

An urban forest population should include no more than



10% of Any ONE species

This 10% threshold has been exceeded by three species:

- Southern Live Oak
- Cabbage Palm
- Laurel Oak



20% of any ONE genus

This 20% threshold has been exceeded by two genera:

- Quercus (Oaks)
- Sabal (Cabbage Palms)



30% of any ONE family

This 30% threshold has been exceeded by two families:

- Fagaceae (Beeches [Oaks])
- Arecaceae (Palms)

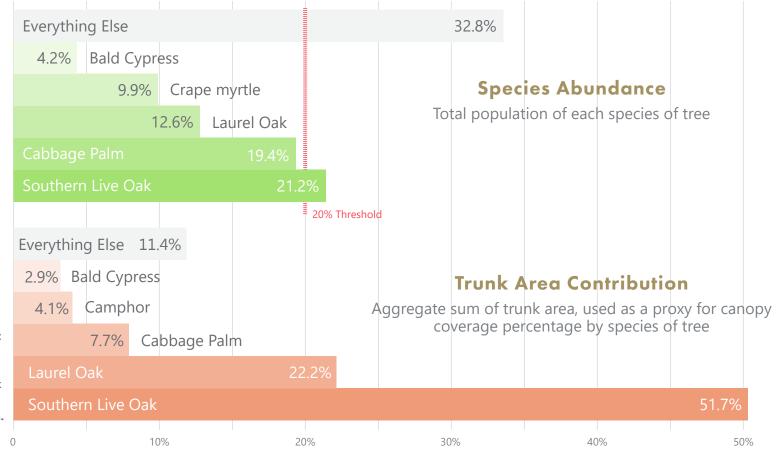


Figure 26 is a graphic representation of the species abundance and trunk area contribution within the Study Area.



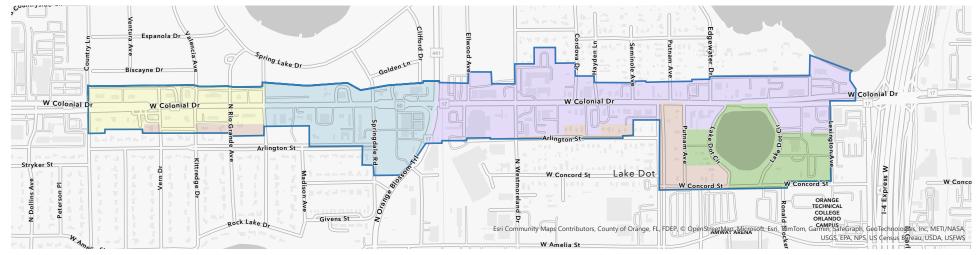


Figure 28 is a graphic representation of the Landscape Zones.

Legend

Landscape Zones

Name

- **Commercial Center**
- **Commercial East**
- **Commercial West**
- Lake Dot
- Transitional
- <all other values>

Landscape Characteristics and Strategies

This urban landscape presents unique challenges and opportunities for tree planting. To create a thriving, resilient urban forest, we must adopt strategic approaches that promote biodiversity, accommodate growth, and address the constraints of urban spaces.

The following recommendations offer a framework for cultivating a sustainable urban canopy that benefits both people and the environment.

Commercial West

- Two lanes, divided by a paved median or turn lane
- Commercial buildings with tiered landscape areas and larger lawn areas
- Promote Identity = Limited Tree Palette
- Room for Large Trees = Space Above and Below
- Preserve Existing Trees

Transitional Areas

- Two-lane residential streets, divided by a median
- Houses with landscaped front yards facing the road
- Roadway medians are four feet wide or greater, often with large mature oaks
- Promote Diversity = Underutilized Species
- Room for Large Trees = Space Above and Below
- Preserve Existing Trees
- Foster Community = Make Trees Fun!

Commercial Center

- Two lanes, divided by a paved median or turn lane
- Commercial buildings with tiered landscape areas and smaller lawn areas
- Promote Identity = Limited Tree Palette
- Pavement Removal = Needs Larger Rooting Area
- Utility Conflicts = Smaller Trees or Limited Planting Areas
- Preserve Existing Trees

Lake Dot

- Open park surrounding Lake Dot with large trees
- Commercial properties on both sides of the lake
- Wide sloping lawn to the lake and other large lawn areas
- Promote Diversity = Underutilized Species
- Room for Large Trees = Space Above and Below
- Attract Wildlife = Less Showy Plants

Commercial East

- Four lanes, divided by a paved median or turn lane
- Commercial buildings with minimal setback from W Colonial Dr, minimal landscaping, sometimes none
- Several vacant lots
- Promote Identity = Limited Tree Palette
- Requires Durability = Heat and Drought Tolerant
- Pavement Removal = Create Larger Rooting Area
- Utility Conflicts = Smaller Trees or Limited Planting Areas

Figure 29 is a graphic representation of an existing tree and sidewalk condition. *Where noted, 1.0x dbh is to mean 1.0 foot per diameter at breast height inch.

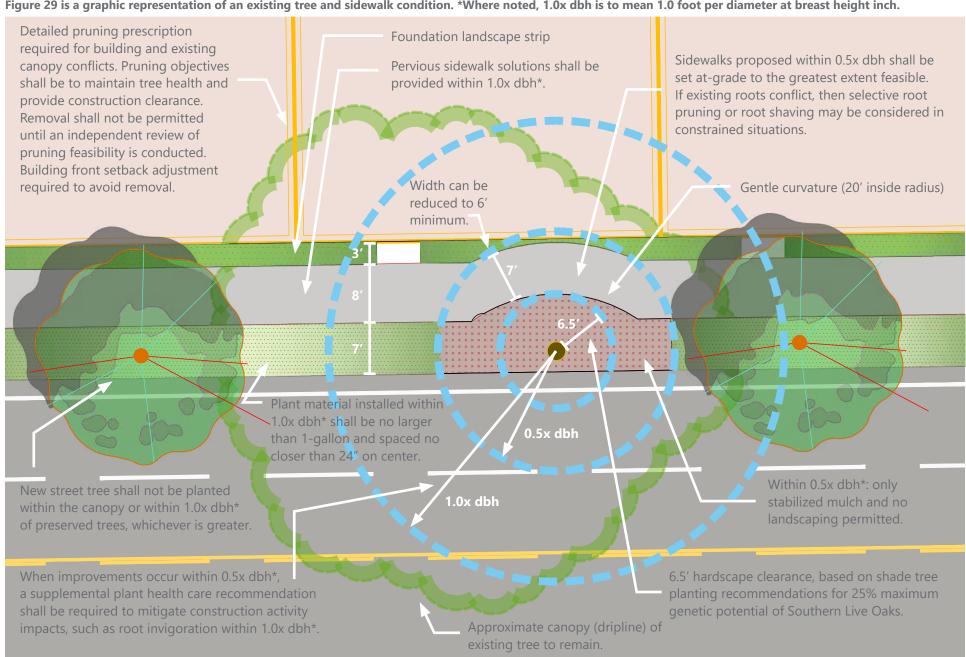
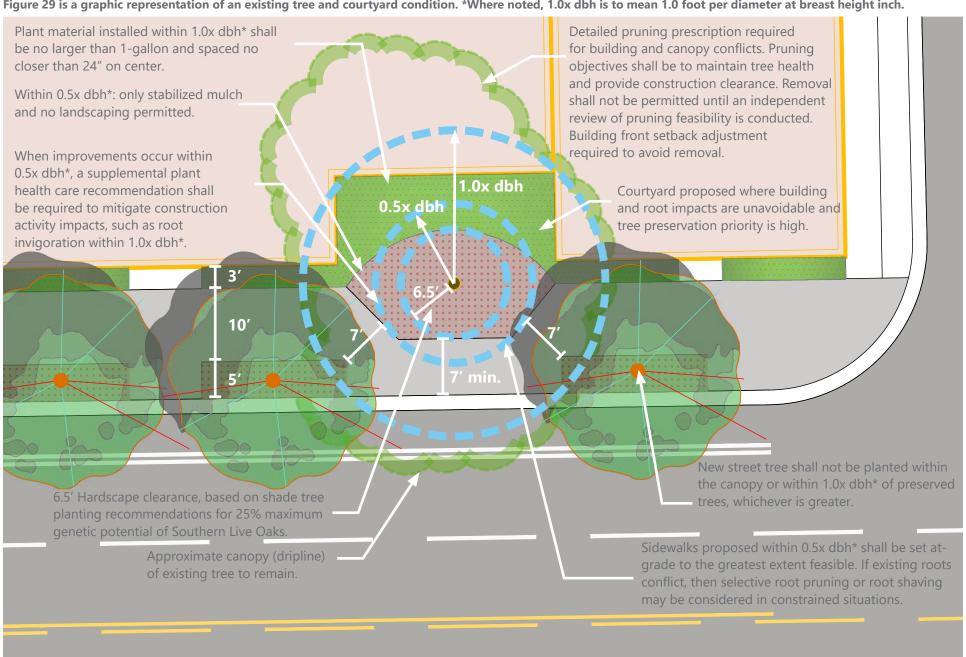
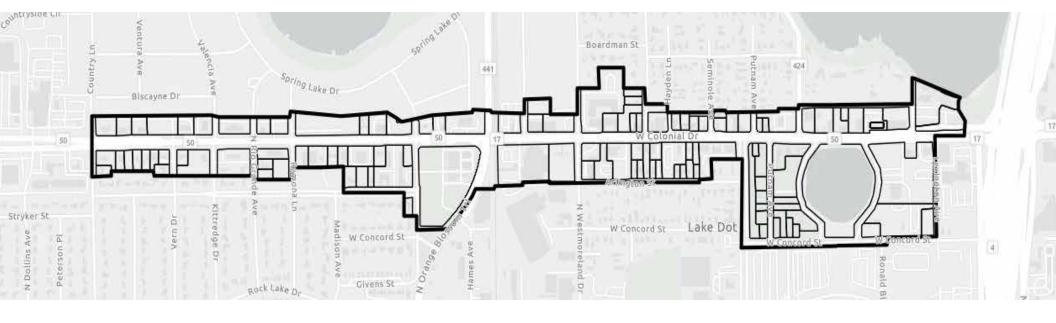


Figure 29 is a graphic representation of an existing tree and courtyard condition. *Where noted, 1.0x dbh is to mean 1.0 foot per diameter at breast height inch.





APPENDICES

Community Outreach Materials



Figure A.1 is a graphic representation of the results from one of the community outreach meeting activities which addressed residents' specific concerns.

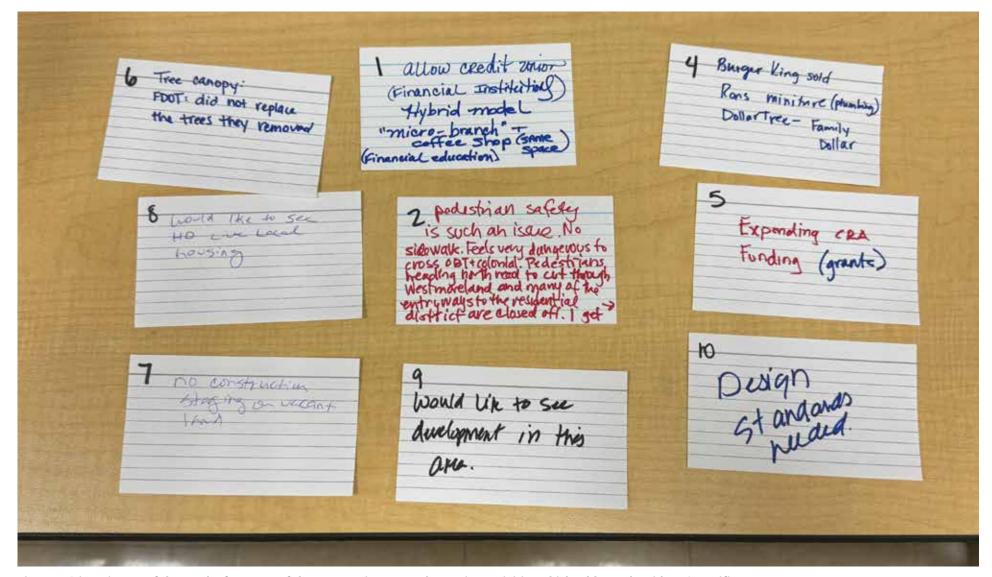
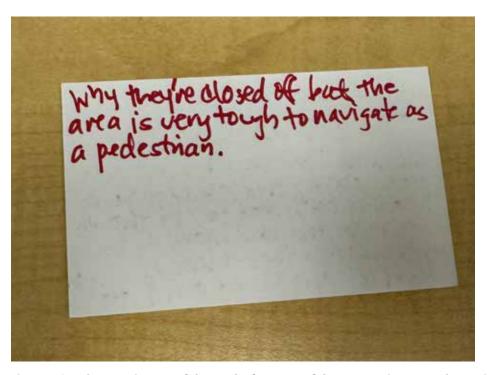


Figure A.2 is an image of the results from one of the community outreach meeting activities which addressed residents' specific concerns.



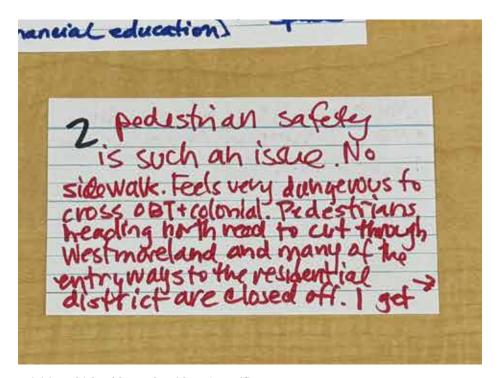


Figure A.3 and A.4 are images of the results from one of the community outreach meeting activities which addressed residents' specific concerns.

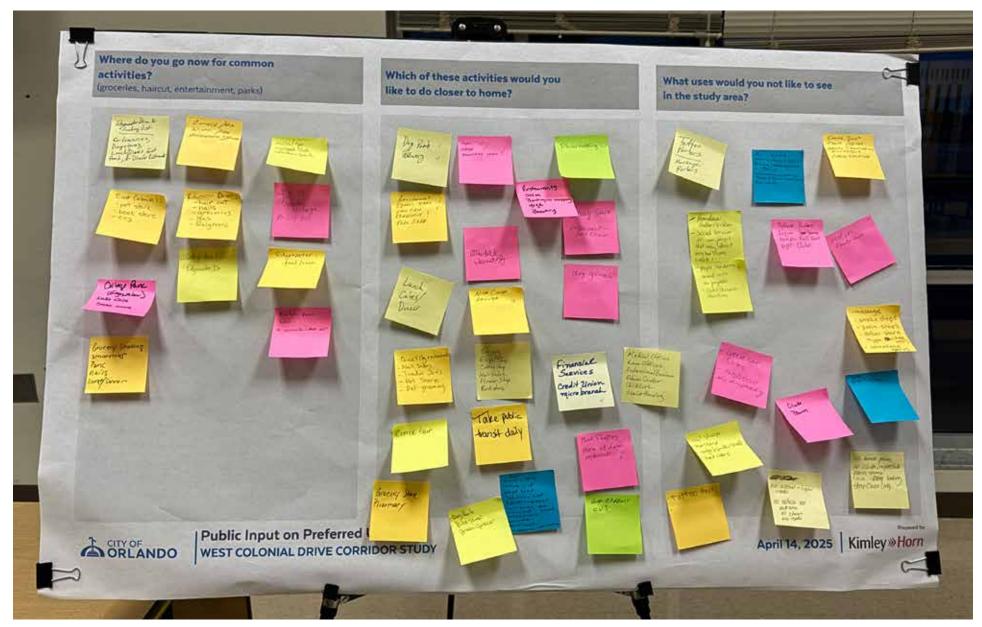


Figure A.5 is an image of the results from one of the community outreach meeting activities which addressed the publics preferred uses.

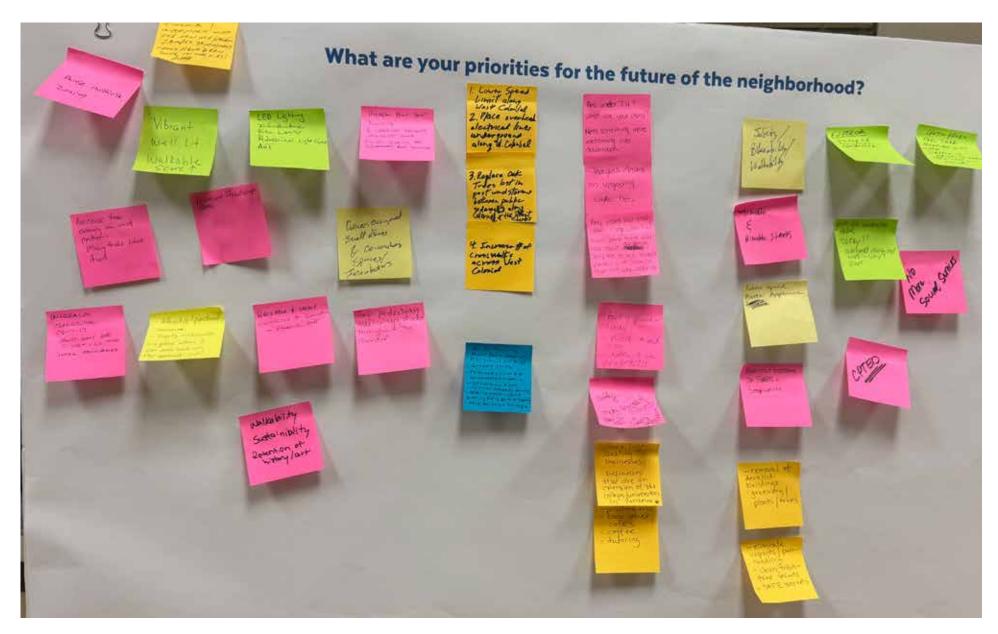


Figure A.6 is an image of the results from one of the community outreach meeting activities which addressed residents' priorities for the study area.

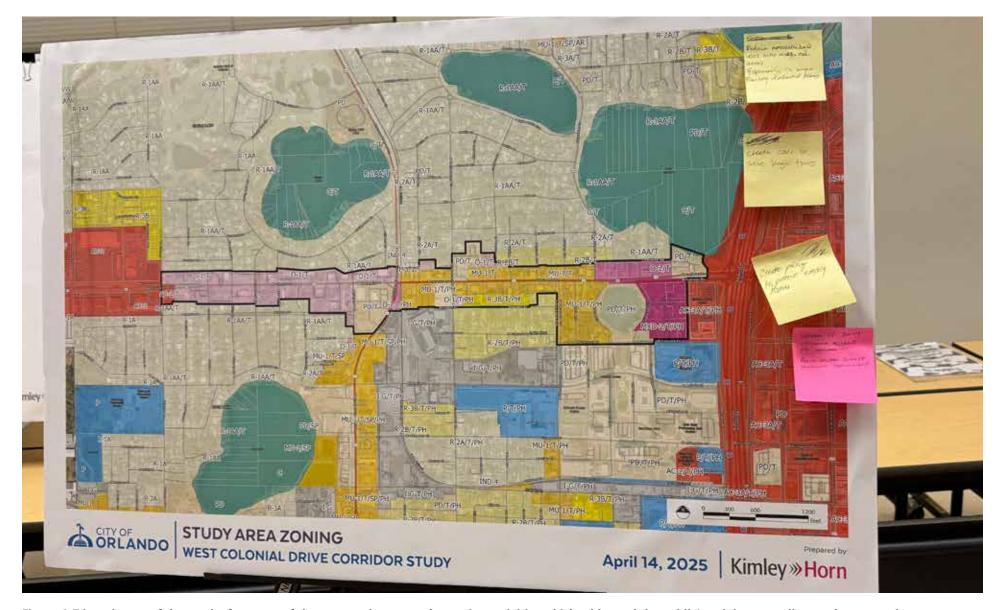


Figure A.7 is an image of the results from one of the community outreach meeting activities which addressed the public's opinion regarding study area zoning.

Prior and Parallel Planning Initiatives

Orlando Cross

The Orlando CROSS Project—short for Connecting Residents on Safe Streets—is a federally funded initiative launched to advance the City of Orlando's Vision Zero goal of eliminating traffic-related fatalities and serious injuries by 2040. Adopted following the city's receipt of a RAISE grant from the U.S. Department of Transportation (USDOT), the project focuses on six high-risk corridors identified through crash data, equity analysis, and community impact assessments.

The CROSS Project aims to:

- Develop a Vision Zero implementation plan tailored to six critical corridors.
- Identify context-specific countermeasures such as safer crossings, sidewalk connectivity, bikeways, and transit access improvements.
- Prioritize equity and community engagement, ensuring under served neighborhoods receive targeted safety upgrades.

Corridors studied:

- Kirkman Road (Conroy to Raleigh)
- SR 50/Colonial Drive (Ferguson to Maguire)
- Orange Blossom Trail (Winifred to SR 50)
- Orange Avenue and Rosalind/Magnolia (SR 408 to SR 50)
- Orange Avenue (Pineloch to SR 408)
- Semoran Boulevard and Curry Ford Road (Grant to Dahlia; Larkin to Bowen)

The ongoing study is conducting a comprehensive corridor analysis that includes crash mapping, equity profiling, and community context assessments. The resulting recommendations will address short-, mid-, and long-term improvements, designed to balance

innovative design strategies with compliance to FDOT standards. These findings are intended to inform and be integrated into broader regional planning initiatives, including MetroPlan Orlando's 2050 Metropolitan Transportation Plan.

Parramore Plan

The Parramore Comprehensive Neighborhood Plan was officially adopted by the Orlando City Council on January 26, 2015. This plan was developed as part of a U.S. Department of Housing and Urban Development (HUD) Sustainable Communities Regional Planning Grant and aimed to create a healthy, sustainable, and vibrant community in Parramore. It was also a key component of the broader Project DTO initiative and built upon the earlier Pathways for Parramore efforts. Streetscape improvements along West Colonial Drive were identified as a long-term action item.

The City established the Parramore Heritage Overlay District on the heals of the Parramore Plan to move forward identified objectives including:

- Establishes setbacks to one and two-family infill lots.
- Applies to sites on southern portion of West Colonial Drive from South Orange Blossom Trail to I-4.
- Prohibits new or expanded social services.

OBT NEXT Master Plan

The OBT Next Master Plan was developed and adopted in 2017 as part of a broader revitalization initiative led by the Orange Blossom Trail Development Board (OBTDB), in partnership with the City of Orlando, Orange County, and the Florida Department of Economic Opportunity (DEO).

The plan was designed to reimagine and revitalize an 8-mile stretch of the Orange Blossom Trail (OBT) corridor historically challenged by disinvestment, crime, and infrastructure neglect.

Its core goals included:

- Economic revitalization: Attracting new investment and supporting local businesses.
- Community safety: Establishing the OBT Safe Neighborhood District, which coordinates monthly safety meetings across agencies.
- Infrastructure modernization: Upgrading sidewalks, lighting, and traffic signals to improve pedestrian safety and accessibility.
- Transit-oriented development (TOD): Encouraging mixed-use development near proposed Bus Rapid Transit (BRT) stations at Westmoreland and OBT.
- Neighborhood connectivity: Enhancing pedestrian and bike access between residential areas and transit stops.

The plan has already led to several tangible outcomes:

- Redevelopment Projects: The former McNamara car dealership site is being transformed into a mixed-use development.
- Infrastructure Enhancements: Phase 2 improvements along South OBT (from 30th Street to Gore Street) include ADAcompliant sidewalks, new mast arm signals, and enhanced lighting. Construction began in January 2025 and is expected to conclude by late summer 2025.
- Community Engagement: Strong public support was documented during outreach efforts, especially for pedestrian safety and redevelopment priorities.
- Integration with Regional Plans: The plan's recommendations were incorporated into the 2050 Metropolitan Transportation Plan by MetroPlan Orlando.

OBT Sidewalk Study

In 2021, the Florida Department of Transportation (FDOT) District 5 conducted a feasibility study to evaluate the potential for a new

sidewalk along Orange Blossom Trail (OBT), between Colonial Drive and Country Club Road. The study proposed an 8-foot-wide sidewalk with landscape buffers on the west side of OBT, and confirmed that no right-of-way acquisition or relocation of the adjacent railroad line would be necessary. Public outreach revealed strong community support for the project. Funding is currently being explored by the City of Orlando and MetroPlan Orlando, with FDOT expected to cover the remaining costs.

Lynx SR 50 BRT Station Area Analysis

The SR 50/UCF Connector Alternatives Analysis, which underpins the Lynx SR 50 BRT Station Area Analysis, was completed and adopted in October 2015 by the Central Florida Regional Transportation Authority (LYNX) in collaboration with MetroPlan Orlando, FDOT District 5, and the City of Orlando.

The analysis aimed to evaluate the feasibility and impact of implementing Bus Rapid Transit (BRT) along State Road 50, connecting UCF to Downtown Orlando and Pine Hills. The core objectives included:

- Identifying transit-oriented development (TOD) opportunities within a 10-minute walk of 13 proposed station areas.
- Enhancing multimodal safety through data-driven analysis of crash patterns and pedestrian infrastructure.
- Supporting economic revitalization and land use compatibility by integrating the plan with the City's Growth Management Plan and zoning districts.
- Improving connectivity and mobility through wider sidewalks, curb extensions, and bicycle infrastructure.

The plan proposed a locally preferred alternative that includes BRT service with stations at key nodes such as Westmoreland Drive, LYNX Central, and Fashion Square Mall, among others.

Short Term Strategies Site Selection and Toolbox

Site Analysis and Consideration

The project team performed a parcel land use analysis to identify the vacant and underutilized parcels immediately adjacent or in close proximity to West Colonial Drive. Vacant parcels are those in which the building value is \$0, often translating into empty lots. Underutilized parcels are those in which the building value is less than 30% of the land value, often translating into parcels used for parking lots, lots with abandoned or dilapidated buildings, or lots with very small structures. This analysis identified a number of private and government-owned parcels that were therefore suitable for site activations.

In addition to the parcels identified in the land use analysis, the project team considered other locations that are considered suitable to support site activations or quick build interventions, such as deadend streets and some street intersections.

Potential Sites for Creative Placemaking

The identified sites were prioritized and selected based on the following eight criteria:

- Ownership:
 - Public
 - Private
- Availability of Parking or Proximity to Parking
- Pedestrian Access
- Ability to leverage Main Street partnerships:
 - West Lakes Properties Market Street
 - Parramore Main Street District
- Site Readiness
- Availability of Shade
- Access to Power and Utilities
- Management of Traffic (MOT) Potential Implications

The following sites were identified as the ones with highest potential for short-term activations. The activation of these sites may have the ability to positively impact neighboring property enhancing neighborhood vitality and potentially triggering economic value. The Placemaking Toolbox outlined in the following pages guides the implementation of these concepts.

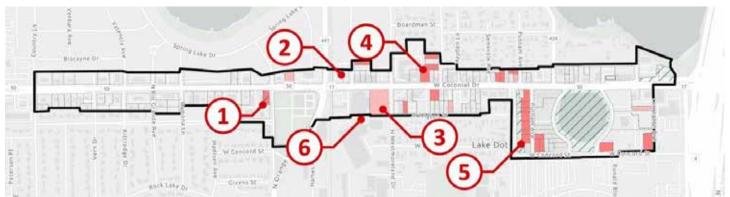


Figure E.1 illustrates the location of the short term revitalization strategy sites within the study area, to be viewed in conjunction with Table E.1.

Site	Site Name / Owner	Location	Ownership	Project Type	
1	City of Orlando	1300 West Colonial Drive	City	Site Activation	
2	Lifted Smoke Shop	1115 West Colonial Drive	Private	Site Activation	
3	McNamara Site	1005 West Colonial Drive	Private	Site Activation	
4	Sun Beach Investment Inc.	911 West Colonial Drive	Private	Site Activation	
5	Meshkati Asghar, et. Al.	608 - 644 Putnam Avenue	Private	Site Activation	
6	Arlington Street	Between Westmoreland Drive and train tracks	City	Site Activation	
6a	Intersection	Arlington Street & Westmoreland Drive	City	Intersection Mural*	
6b	Intersection	West Colonial Drive & Westmoreland Drive	City	Intersection Mural*	

Table E.1 lists the six sites determined apt for a short term revitalization strategy, to be viewed in conjunction with Figure E.1.

^{*}This was drafted prior to the release of the USDOT memorandum on July 1, 2025 and that the artistic portion of the intervention may not be permitted due to changes in FDOT policy resulting from the federally issued memorandum. https://ops.fhwa.dot.gov/memorandum/Governors_Safe_Roads.htm

Placemaking Toolbox

This section presents a sample palette of materials that can be used as guidance to implement the concepts for the Arlington Street Plaza and West Colonial Park site activations. For temporary placemaking projects, procurement of materials often involves creativity and collaboration. Rather than relying solely on purchased materials, leverage partnerships to access recycled materials wherever possible.

This Placemaking Toolbox groups materials into five categories by their function:

- Traffic Barrier Elements
- Surface Elements
- Main Elements
- Programming Elements
- Wayfinding Elements

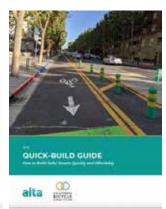
For extended reference, explore the following guidance documents:

- City of Orlando Quick Build Guide
- Tactical Urbanist's Guide to Materials and Design
- AARP's Pop-Up Placemaking Toolkit
- California Bicycle Coalition's Quick Build Guide
- LADOT's People St. Kit of Parts for Plazas

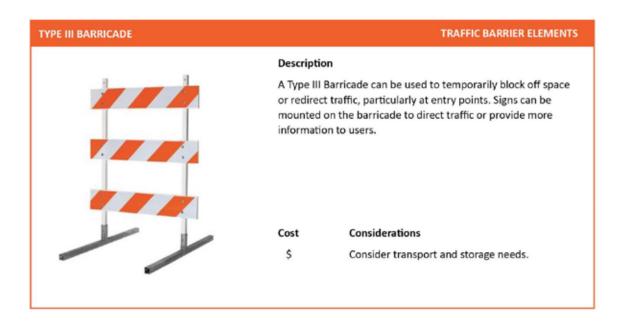






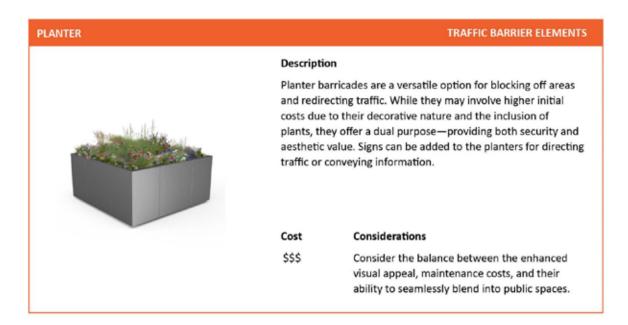








Description A concrete Jersey barrier is ideal for long-term use when blocking off areas or redirecting traffic at entry points. They offer superior durability and stability compared to other temporary options. Signs can also be mounted on these barriers for traffic control or to provide additional information. Cost Considerations \$\$\$\$\$ Consider the higher upfront cost and logistical challenges, but also the long-term durability and minimal maintenance requirements.



TEMPERA PAINT De re

SURFACE ELEMENTS

Description

Tempera paint is a low-cost material that can be used to stripe very short-term asphalt murals and demonstrations when fast removal is a concern.

Cost Considerations

\$ Consider the longevity of the paint against the duration of the temporary project.

SPRAY PAINT *

SURFACE ELEMENTS



Description

Spray paint can be used to fill in areas with asphalt murals. It can be combined with stencils for an easy installation. Power washing can be used to manually remove spray paint on a shorter timeline to leave the space back to normal conditions.

Cost Considerations

\$ Consider the longevity of spray paint against the duration of the temporary project.

^{*}This was drafted prior to the release of the USDOT memorandum on July 1, 2025 and that the artistic portion of the intervention may not be permitted due to changes in FDOT policy resulting from the federally issued memorandum. https://ops.fhwa.dot.gov/memorandum/Governors_Safe_Roads.htm

Description Acrylic traffic paint can be used to fill in areas with asphalt murals, especially at intersection and crosswalk murals. It can be combined with stencils for an easy installation. Reflective glass beads can be added for longer-term applications to increase retro-reflectivity and skid resistance. Cost Considerations \$\$\$\$ Acrylic paint is expected to last 1-3 years before requiring re-application.



1ULCH SURFACE ELEMENTS



Description

Mulch is a simple, effective placemaking element used to create defined areas, such as playgrounds or garden paths, while adding a natural aesthetic to the space. It helps with soil moisture retention and weed control, making it ideal for green spaces.

Cost	Considerations
\$	Select mulch that's non-toxic and safe for children and pets. Consider drainage and plan for regular maintenance to replenish it.

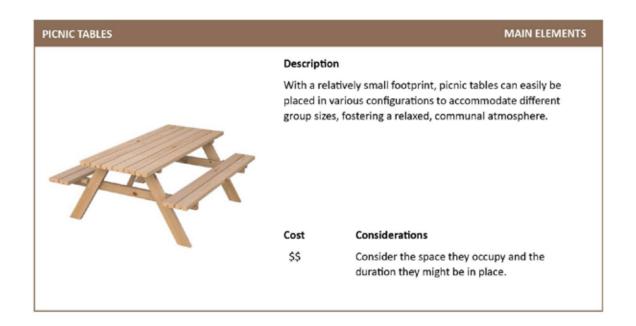
FOOD TRUCKS MAIN ELEMENTS

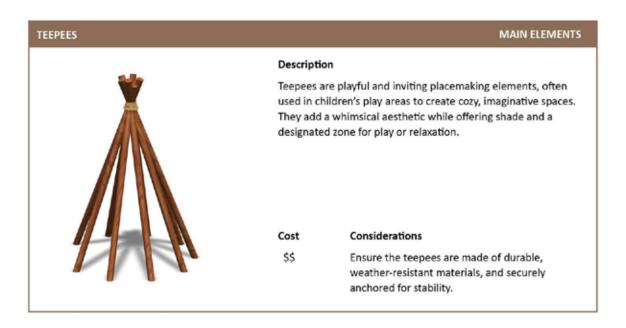


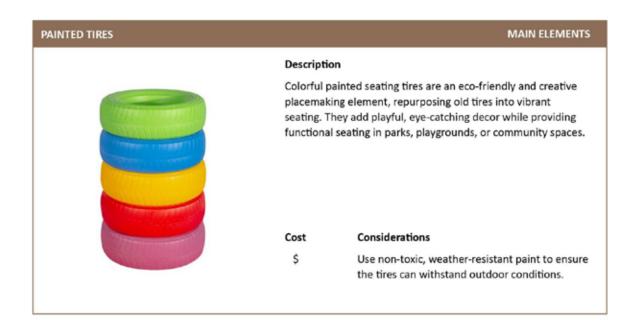
Description

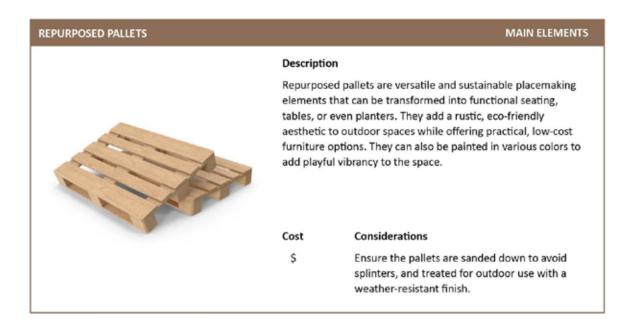
Food trucks are versatile placemaking elements that can activate public spaces for short-term events or seasonal installations, bringing energy and vibrancy to plazas, parks, or streets. Their compact size allows them to fit into various urban settings without permanent infrastructure, making them ideal for both temporary activations and rotating locations. Beyond providing diverse culinary offerings, they foster social interactions and community engagement.

Cost	Considerations
N/A	Consider the variety and meaning of the menu
	offerings. Coordinate with vendors.

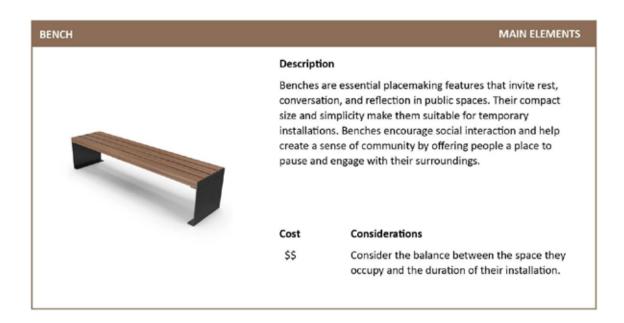












UMBRELLA CANOPY

MAIN ELEMENTS

Description

A colorful umbrella canopy is a striking placemaking installation that transforms public spaces into vibrant, visually engaging environments. Typically used for temporary events or seasonal activations, this overhead display of umbrellas not only adds shade and shelter but also creates a playful, inviting atmosphere. Ideal for pedestrian streets, plazas, or markets, it draws people in, encouraging exploration and interaction.

Cost Considerations

\$\$ Consider the visual impact and the space it covers.

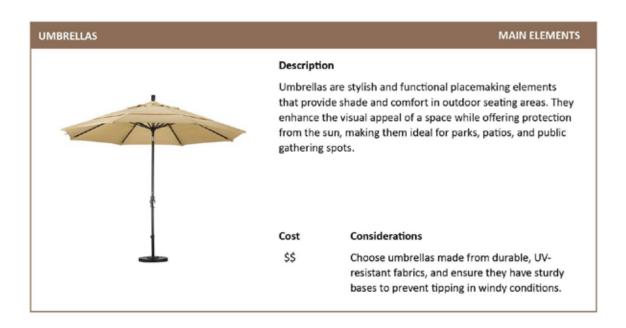
MURAL ART MAIN ELEMENTS

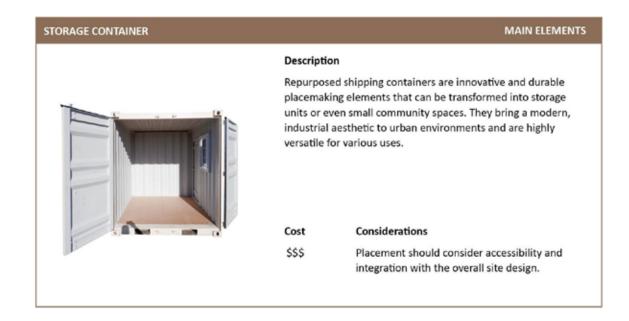


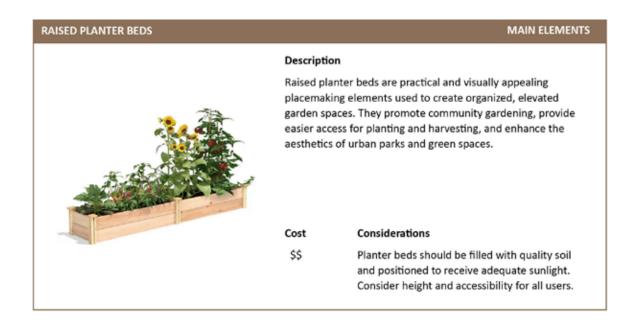
Description

Mural art is a powerful placemaking element that can transform blank or overlooked walls into captivating works of art. Whether temporary or permanent, murals reflect local culture, history, or values, fostering a sense of community pride and belonging. Their large scale makes them ideal for activating streetscapes, alleys, or plazas, creating visually engaging environments that encourage people to linger and explore.

Cost	Considerations
\$\$\$	Consider its cultural relevance, the space it
	occupies, and its lasting impact.











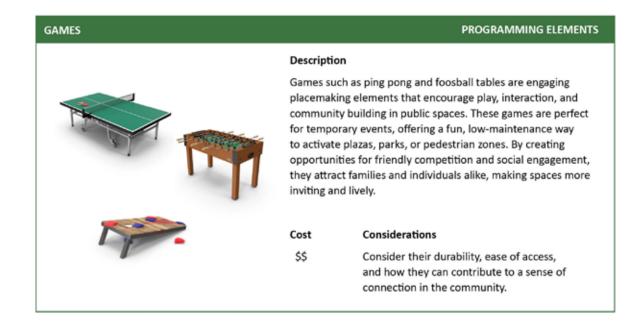


MOBILE STAGE

PROGRAMMING ELEMENTS

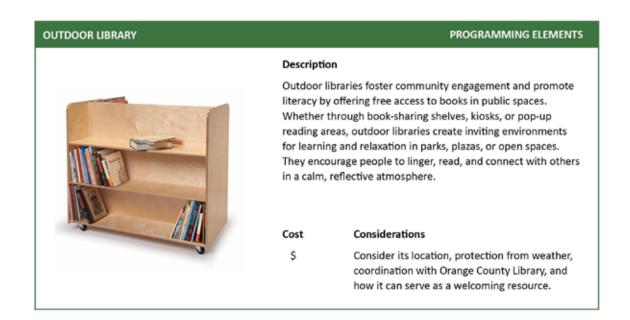
A mobile stage is a versatile placemaking tool that brings entertainment and cultural activities to various public spaces, transforming parks, streets, or plazas into vibrant event venues. Ideal for temporary installations such as concerts, performances, or community gatherings, mobile stages provide flexibility in terms of location and event duration. They can quickly activate underutilized areas, fostering social interaction and community engagement.

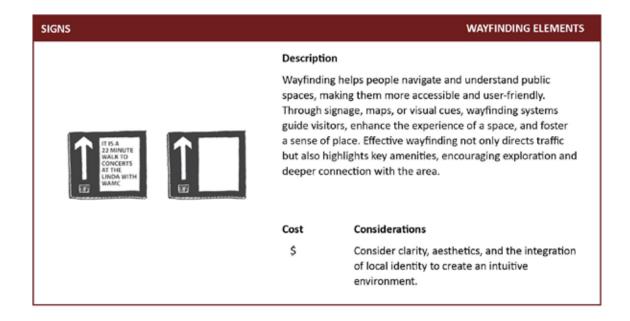
Cost	Considerations
\$\$\$	Consider its size, setup requirements, and how it can adapt to the environment.



Description

OUTDOOR FITNESS	PROGRAMMING ELEMENTS	
	Description	on
	Outdoor fitness classes are dynamic placemaking elements that transform open spaces into active, health-focused hubs. These classes encourage physical well-being and social interaction, drawing people together for shared experiences. Perfect for temporary activations, they bring energy and a sense of community to underutilized spaces.	
	Cost	Considerations
	N/A	Consider the space requirements, the timeframe for use, and coordination with the class provider.





Live Local Act

Originally adopted in 2023, the Live Local Act (SB 102) was passed through the Florida legislature to address a deficit of attainable housing in the state. In May 2024, Senate Bill 328, which amended

the original Live Local Act, was signed by the Governor. In June 2025, the legislation was further amended when the Governor signed SB 1730, which went into effect on July 1, 2025. This analysis was completed based on the 2023 and 2024 legislation. Additional legislation in 2025 may alter the findings of this section.

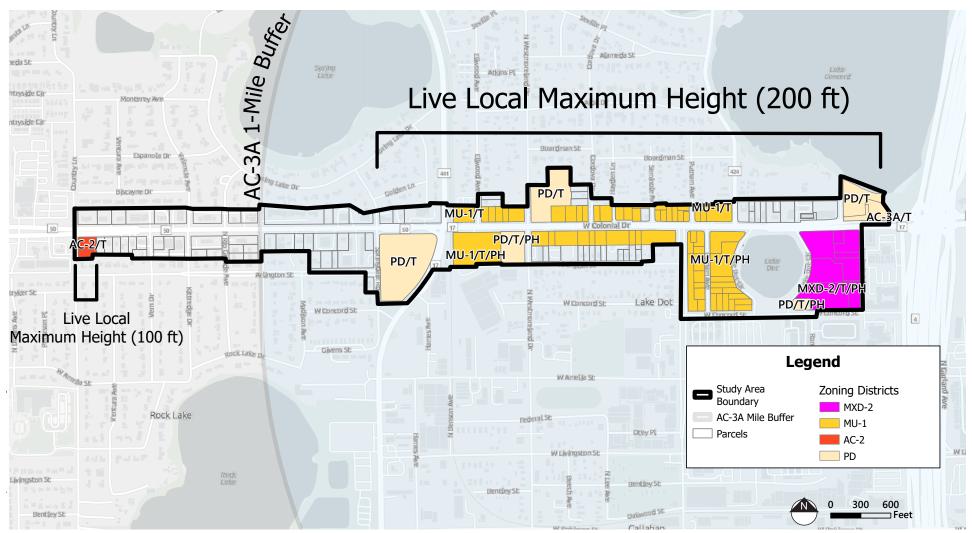


Figure G.1 This map analyzes the zoning districts elligible under the Live Local Act.

The Act preempts aspects of local zoning and land use regulations on certain properties, including restrictions on height, floor area ratio, density, use, and parking for qualifying multi-family developments. The focus of this memo is to assess the potential permitted heights in the study area if a parcel were to develop under the Live Local Act.

According to the Act, a qualifying development may be eligible for the maximum allowed height for a commercial or residential building within a 1-mile radius, or 3 stories, whichever is higher. In the 2024 amendment of the Act (SB 328), additional height restrictions may apply if the parcel is adjacent to a single-family residential use on more than one side. Below is language regarding adjacency to single-family residential development:

"If the proposed development is adjacent to, on two or more sides, a parcel zoned for single-family residential use which is within a single-family residential development with at least 25 contiguous single-family homes, the county may restrict the height of the proposed development to 150 percent of the tallest building on any property adjacent to the proposed development, the highest currently allowed height for the property provided in the county's land development regulations, or 3 stories, whichever is higher. For the purposes of this paragraph, the term "adjacent to" means those properties sharing more than one point of a property line but does not include properties separated by a public road."

According to the City of Orlando's interpretation of the Live Local Act, the following zoning districts are identified as eligible for Live Local development:

- Mixed Use Corridor Districts: MU-1, MU-2
- Activity Center Districts: AC-1, AC-2, AC-3, AC-3A, AC-N,
- Industrial Districts: I-C, I-G, and I-P

Potential Live Local Maximum Heights Methodology

Based on the interpretation of the Act by the City, the parcels within the study area with applicable zones were identified: AC-2, MU-1, MXD-2, and PD. Parcels zoned O-1, O-2, and R-3B are excluded from this analysis.

To determine the maximum "Live Local" height for the parcels within the study area, the zoning district within 1 mile with the highest maximum height, AC-3A, was analyzed. A 1-mile radius of the from the Downtown AC-3A zoning district boundary was mapped. The buffer extends west past Orange Blossom Trail, encompassing every subject parcel within the study area except for one at the far west of the study. This is displayed in Figure 21. The maximum permitted height for district AC-3A is 200 feet.

The parcels labeled AC-2 on the west edge of the study area do not fall under the buffer. The allowable height on these parcels would not be affected by the Live Local Act because the maximum height in AC-2 is equal to or greater than other parcels within a one-mile radius of the location. Therefore, these AC-2 parcels would retain the maximum permitted height of 100 feet whether or not they were developed under the Live Local Act.

Figure G.2 shows existing residents land uses within and near the study area. There are three Live Local eligible parcels in the study area that abut an existing single-family use on two sides, shown in blue. Those parcels are 710 West Colonial Drive, 734 West Colonial Drive, and 744 West Colonial Drive. However, these single-family parcels do not appear to be part of a "25 contiguous single-family homes" subdivision. Based on this analysis, the added height restrictions from the 2024 amendments to the Live Local Act are not expected to apply to these parcels. Based on this interpretation, all parcels in the study area subject to the Live Local Act would be eligible for the maximum allowable heights permitted within one mile as described above.

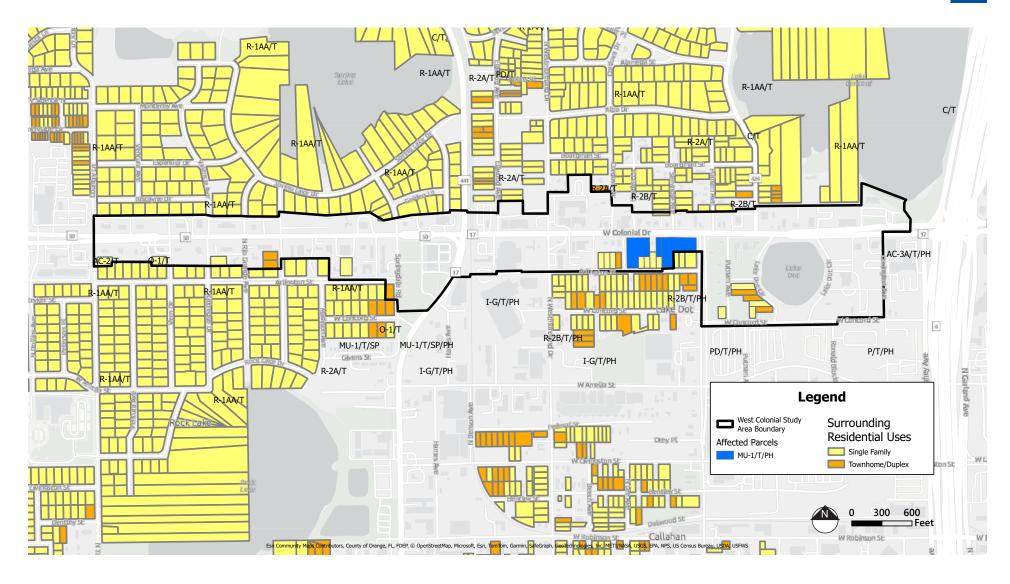


Figure G.2 is a map showing the residential uses within the study area.