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# DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

September 2018





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**CITY OF ORLANDO**

**SODO DISTRICT  
DEVELOPMENT STANDARDS AND GUIDELINES**

September 2018

Prepared by



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# SODO DISTRICT DEVELOPMENT STANDARDS

## A. OVERVIEW

According to the South Downtown Vision Plan, “The South Downtown Area is intended to be a vibrant, recognizable, mixed-use, multi-modal neighborhood that thrives on the synergies afforded by the continued growth of the Orlando Health campus and adjacent medical related uses.” The area addressed in the South Downtown Vision Plan extends from I-4 and Orange Avenue and from SR 408 to Michigan Avenue. The great majority of the area is already developed with a combination of urban and suburban residential, commercial, medical and industrial uses. The city intends to continue encouraging that diversity of uses, but to guide future development into the creation of an urban form with higher density and intensity uses that respect the pedestrian scale, emphasizes active ground floor active uses, and facilitate the provision of workforce housing.

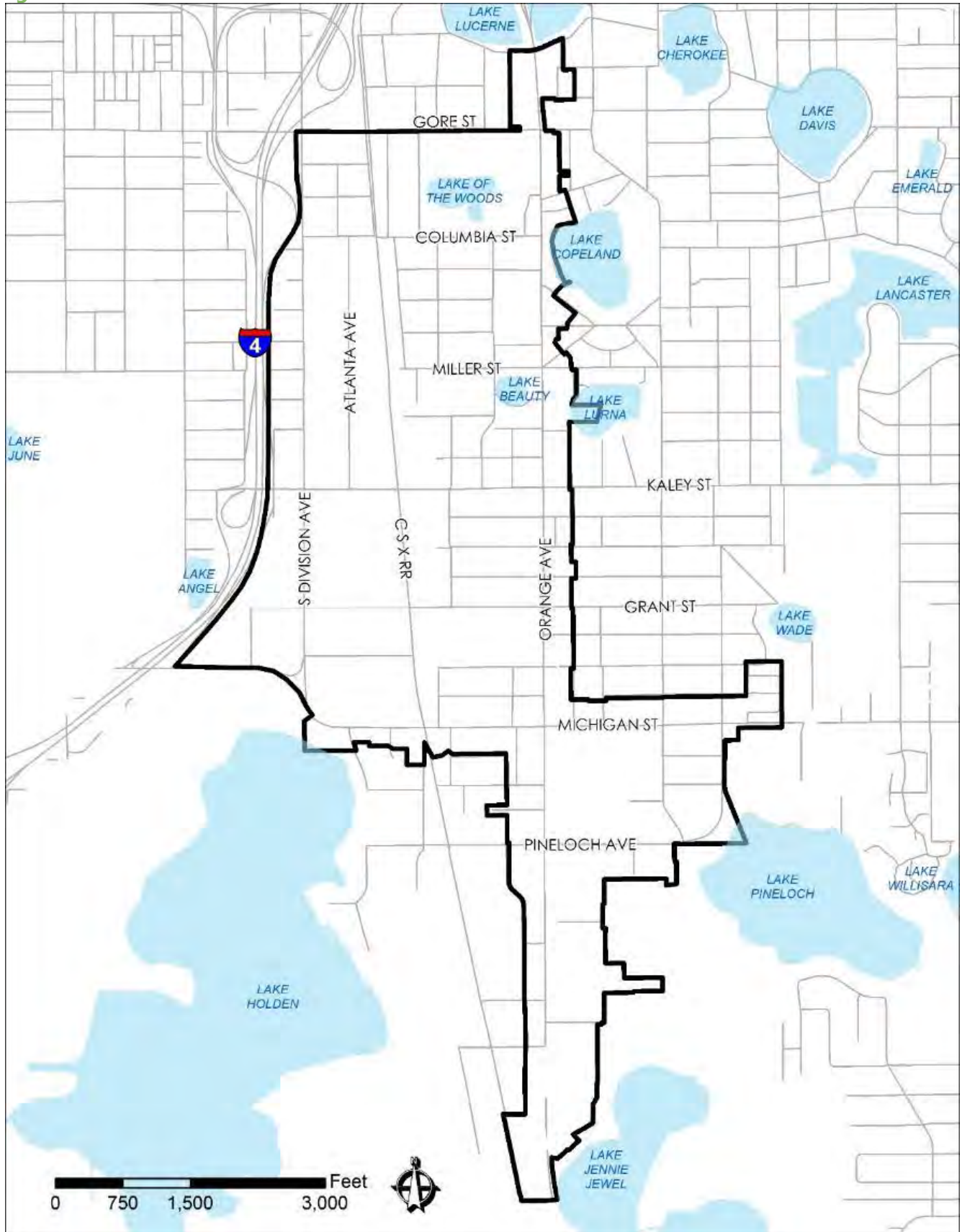
The Orange/Michigan Special Plan overlay district, which covers the Orange Avenue and Michigan Street corridors, was developed to preserve and strengthen commercial activities and redevelopment opportunities within those corridors, enhance pedestrian safety and main street character, and protect existing residential neighborhoods from encroachment.

This document contains development regulations, with an illustrated guide of those regulations, applicable to the two planning areas combined (referred to in this document as the SODO District). The intent of the regulations is to:

- Facilitate infill and guide future growth while protecting the existing residential neighborhoods within and adjacent to the district.
- Encourage design diversity and variation, while ensuring a compact and dense urban form concentrated on the west side of the Orange Avenue corridor with a gradual transition to the less dense residential neighborhoods to the east of the Orange Avenue corridor.

The adoption of the SODO District proposed in this document will fulfil Policy 12.7 of the Growth Management Plan, which states. “To implement the recommendations of the Vision Plan [South Downtown Vision Plan], the City may adopt one or more Special Plan Overlay zoning designations.” For the purpose of this document, the SODO district extends from Interstate 4 to just east of Orange Avenue, and from Lake Lucerne and Gore Street to the south city limits (see Figure 1).

The South Downtown Area is intended to be a vibrant, recognizable, mixed-use, multi-modal neighborhood that thrives on the synergies afforded by the continued growth of the Orlando Health campus and related medical uses, and the continued success of the SoDo Main Street District.

**Figure 1. SODO District Form-Based Code Area**

## B. DEFINITIONS

The following definitions are provided to complement the definitions found in the Land Development Code.

**Fenestration:** The proportion of glass on doors and windows to the surface of a building façade.

**Mixed-Use Development:** For a development to be classified as mixed-use it must include at least two of the following uses: multi-family, office, retailing, personal service, eating & drinking, hotel, indoor recreation, public benefit use or child care center. The secondary use, however, must comprise at least 10% of the building area and may not be reserved for use only by the principal user (for example, a residents-only gym or an employee cafeteria).

**Parking, Off-street:** An independently accessible off-street storage space, either outdoor or within a structure, for the parking of motor vehicles.

**Push-Thru Lettering:** A transparent or translucent letter system which is "pushed through" a routed aluminum sign face that may be combined with a vinyl overlay, on the front or back, to add color.

**Streets, Primary (SODO):** Orange Avenue, Michigan Street, Kaley Avenue, Gore Street, Grant Street, Pineloch Avenue, Division Street, Delaney Avenue, Miller Street, and Columbia Street (see Figure 74).

**Streets, Secondary (SODO):** All streets within the district not classified as "Primary Streets" (see definition of Primary Streets above).

**Street Side Setback (SODO):** A yard extending across the side of a corner lot between the rear line of the front yard and the front line of the rear yard, and between the principal building and the back of the sidewalk.

**Street Setback Zone (SODO):** Street Setback Zone refers to the area between the principal building and the back of the sidewalk along the front and street sides of the lot.



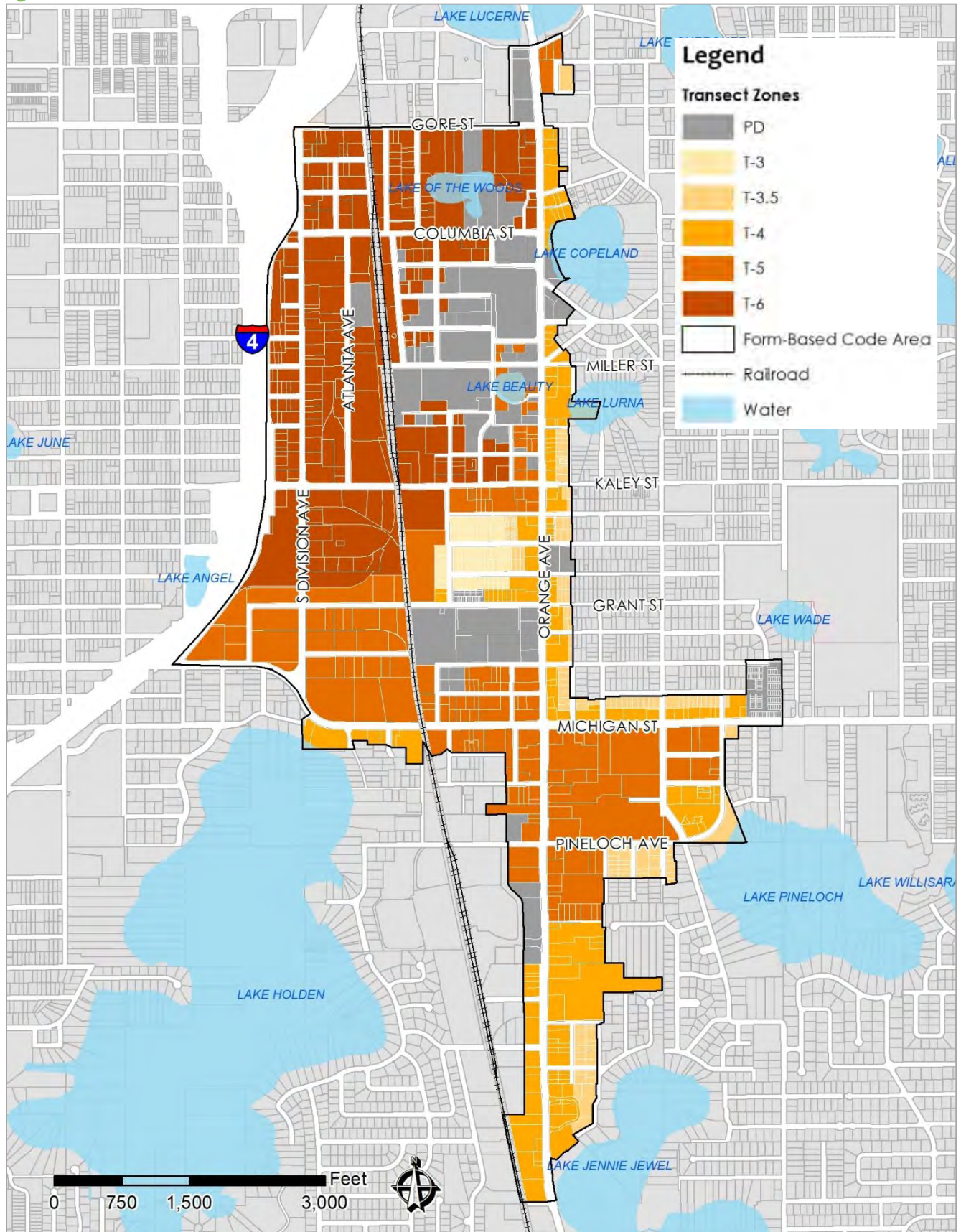
## C. REGULATING PLAN

Figure 2 shows the five (5) transect zones that will implement the vision for the SODO district. They were defined based on development trends and the desired vision for the area. They include:

- T-6, Urban Core
- T-5, Urban Center
- T-4, General Urban
- T-3.5, Neighborhood Transition
- T-3, Neighborhood

The following pages contain a description and vision for each transect zone. Properties zoned planned development (PD) already have defined entitlements and are therefore excluded from the transect zone designation.

Figure 2. SODO Transect Zones





**T-6****URBAN CORE****1. T-6, URBAN CORE**

The primary intent of this zone is to allow redevelopment to create a vibrant, transit-oriented neighborhood with a mix of residential, retail and office uses while continuing to allow the type of industrial activities that have resided in this area for many years. This zone will focus on allowing high intensity development that supports public transit users as well as bicycle and pedestrian activity. The T-6 zone is also intended to accommodate uses that typically locate close to a hospital.

Ground-floor commercial uses are encouraged to generate activity and contribute positively to a pedestrian-friendly environment, but frontages may also be occupied by other uses such as hotels, multifamily, public or office uses. Buildings are to be placed close to the street, but exceptions are provided to accommodate outdoor gathering spaces (e.g. public plazas, outdoor dining) as well as pedestrian activity and safety. Off-street parking shall be located behind the buildings or in covered garages.



The Kaley Street/Division Avenue node is intended to become a highly visible and memorable node of activity representing the western gateway to the SODO district. The area to the north of this gateway is intended to promote a complementary mixture of transit-supportive uses at higher densities and intensities within walking distance of the SunRail station.

Development density, intensity and height is the most intensive in this area, given the presence of the SunRail station within the zone, and its location between the Interstate (I-4) on the west and already intensive hospital uses on the east side.





**T-5****URBAN CENTER****2. T-5, URBAN CENTER**

The primary intent of this zone is to create a vibrant mixed-use place for residents and visitors alike that is pedestrian-oriented and provides opportunities for a mixture of compatible land uses such as residential, commercial, retail, office, and entertainment uses. The design of sites in the T-5 zone should allow residents and visitors to walk between shops and buildings by creating an internal, pedestrian-oriented street network; off-street parking shall be located behind the buildings and supplemented with on-street parking. While vertical mixed-uses are preferred, horizontal mixed-uses and single-use commercial, office or residential buildings shall be allowed if they are designed to conform to the urban form designated for this area. Open space within this Zone shall be provided in the form of amenity areas and public plazas that are urban in character.

It is important to accommodate public transit users, as well as bicycles and pedestrians traveling along the corridor. Ground-floor commercial uses are encouraged to contribute positively to a pedestrian-friendly environment, but frontages may also be occupied by other uses such as hotels, multifamily, public or office uses. Buildings are to be located close to the street but still allow ample room for



pedestrian activity, public gathering spaces and safety. Development in the T-5 zone is still intensive but in a lesser degree than the T-6 zone, serving as a transition between the tallest and most intensive development around the train station and the hospital and the residential neighborhoods south of Michigan Street and east of Orange Avenue.





T-4

## GENERAL URBAN



### 3. T-4, GENERAL URBAN

The T-4 zone accommodates mixed-use development primarily along the east side of Orange Avenue and the north side of Michigan Street east of Orange Avenue, serving the residential neighborhoods to the east and the expected high density residential areas to the west. It also applies to areas on both sides of Orange Avenue south of Butler Drive and the south side of Orange Avenue west of the railroad.

This transect also serves as a buffer between the higher intensity development in the T-6 zone on the northwest quadrant of the district and the low density/historic neighborhoods east of Orange Avenue and the residential areas south of Michigan Avenue. The T-4 zone is intended to include a range of urban uses (commercial, office, service and residential) in a medium intensity urban setting. Horizontal mixed-uses shall be allowed, but vertical mixed-uses are preferred. The pedestrian orientation of the buildings is required.







**T-3.5****NEIGHBORHOOD TRANSITION****4. T-3.5, NEIGHBORHOOD TRANSITION**

The Neighborhood Transition Zone is intended to preserve the residential character of the areas just east of the T-4 corridor along Orange Avenue. The T-3.5 zone accommodates a mix of townhomes, attached or semi-detached residential buildings (1 to 4 dwelling units typical), offices, and residential-office mixed use buildings.

All buildings in this T zone are to be designed to appear residential from adjacent residential streets. No new garages or parking areas are allowed facing the single-family homes in the adjacent areas and are therefore required to be rear-loaded or accessed from adjacent T-4 properties. Building height, setback and vehicular access provisions have been established to achieve compatibility with adjacent established single-family home districts.







**T-3****NEIGHBORHOOD****5. T-3, NEIGHBORHOOD**

The primary intent of the T-3 zone is to protect the single family residential character and scale of the neighborhood north of the SoDo development, west of Orange Avenue. This area is also intended to serve the need for affordable housing near employment. Therefore, only single-family detached residential homes shall be allowed.

Building height and setback provisions have been established to achieve compatibility with adjacent established single-family home districts.

The City's Growth Management Plan has an adopted sub-area policy committing to the preservation of this area (see Subarea Policy S.12.1).





## **D. APPLICABILITY**

The standards and guidelines contained in this document apply to new development and redevelopment within the SODO district depicted in Figure 1. The standards and guidelines are not intended to exclude any use listed as allowed in each zone. However, all proposed developments are required to meet the standards.

### **1. NEW DEVELOPMENT.**

All new development within the SODO district shall fully comply with the standards and guidelines contained in this document and the Land Development Code.

### **2. SUBSTANTIAL IMPROVEMENT.**

Existing developments undergoing a substantial improvement are required to meet all the requirements of the code and the guidelines contained in this document. Substantial improvement is defined in LDC Section 602.200 as any repair, reconstruction, rehabilitation, or improvement of a structure over the preceding 5 years when the actual and aggregate cost of the improvements or repairs of the structure equals or exceeds 50% of the present replacement value of the structure. See also Section F of this document for modification of standards applicable to development within the SODO district.

### **3. CHANGE IN USE.**

Changes in use that go to a higher intensity use classification listed in Figure 2, Chapter 58, or have a higher parking space requirement shall comply with the parking regulations contained in the LDC.

### **4. NON-CONFORMING LOTS, STRUCTURES AND USES.**

Any redevelopment involving a non-conforming lot, structure or use shall be conducted per the requirements of Chapter 58, Part 7 (Nonconformities and Discouraged Uses) of the City or Orlando Land Development Code. That section generally allows non-conforming lots, uses and structures to continue in existence/operation, but restricts their expansion and change to another non-conforming situation.

### **5. RELATIONSHIP TO THE LAND DEVELOPMENT CODE (LDC).**

Provisions not specifically mentioned in this document but addressed in the City Land Development Code shall apply to properties within the SODO district. In the event of any conflict between the provisions of this code and other provisions of the Land Development Code, the provision of this document shall prevail, when identified as a supplemental regulation with a (+) sign.



## E. DEVELOPMENT REVIEW PROCEDURES

All new development and redevelopment within the SODO district shall be subject to review by the SODO TDRC through the Master Plan review process or through Final Site Plan review if Master Plan approval is not required.

### 1. SODO TECHNICAL DEVELOPMENT REVIEW COMMITTEE (+)

The SODO Technical Development Review Committee (SODO TDRC) shall be established to review proposed developments within the SODO district. The committee shall include the following staff representatives or their designees:

- a. Planning Official
- b. Public Works Director
- c. Permitting Services Division Manager

### 2. MASTER PLAN REVIEW

Master Plan review is intended to provide an initial review of a proposed development and its basic development concepts prior to proceeding with a subdivision plat and/or final site plan review (see LDC Chapter 65, Part 2H). The level of information required for this review is general, but with enough detail to demonstrate compliance with the standards of the SODO district. Proposed developments that meet one or more of the following criteria are required to undergo administrative or formal Master Plan review.

Administrative Master Plan	Formal Master Plan
<ul style="list-style-type: none"> <li>• 5,000 to 65,000 sq. ft. of non-residential floor area, outdoor storage space or outdoor merchandise display space</li> <li>• 12 to 100 multi-family units</li> <li>• More than one phase</li> <li>• Drive through or gas pump</li> <li>• Administrative Modification of Standards</li> </ul>	<ul style="list-style-type: none"> <li>• &gt;65,000 sq. ft. of non-residential floor area, outdoor storage space or outdoor merchandise display space</li> <li>• &gt;100 multi-family units</li> <li>• &gt;3 stories</li> <li>• &gt;5 acres</li> <li>• <math>\geq</math>180 parking spaces or parking garage</li> <li>• Non-Administrative Modification of Standards</li> <li>• Density/Intensity Bonus</li> </ul>

### 3. OTHER APPLICATIONS

In cases where the development involves a Conditional Use Permit or a rezone, it shall also be reviewed as directed in LDC Chapter 58, Parts 2D and 2I, respectively.

## **F. MODIFICATIONS (+)**

### **1. MASTER PLAN MODIFICATIONS**

Section 65.334-1 of the LDC provides for a mechanism to approve modifications through the master plan review process. In addition to the types of modifications listed in that section, development within SODO may also obtain a modification to the following standards contained in this document:

- a. Section H, Block & Lot Subdivision Standards
- b. Section I, Site and Building Design Standards
- c. Section J, Access, Circulation and Parking Requirements
- d. Section K, Landscape, Buffering and Screening
- e. Section M, Signs

### **2. ADMINISTRATIVE MODIFICATIONS**

Section 65.330 of the LDC provides for a mechanism to approve modifications administratively. The system is intended to promote high standards of site design, provide flexibility in the administration of standards in recognition of site specific conditions, and to establish conditions to ensure compatibility where standards are modified. In addition to the types of modifications listed in that section, development within SODO may also obtain a modification to the standards listed above in Section F.1.

## G. LAND USE STANDARDS

### 1. ALTERNATIVE USES. (+)

The permitted uses within the SODO district shall be as prescribed for the underlying zoning district. However, the SODO TDRC may approve the alternative uses listed below, provided that the use is consistent with the Future Land Use designation of the site. The second column in Table 1 indicates if the use is subject to supplemental standards included in the LDC or in this document.

Whenever there is any uncertainty as to the classification of a use, the Planning Official shall determine the classification, if any, within which the use falls, based on its characteristics and similarity to other uses in the zone. If a use has characteristics similar to more than one classification, the use shall be construed as the classification having the most similar characteristics. If a particular use is determined not to be within an allowed defined use, then the particular use shall be prohibited.

**Table 1: Alternative Uses Allowed in the SODO District.**

USES	See LDC Section*	T-3	T-3.5	T-4	T-5	T-6
<b>RESIDENTIAL</b>						
Attached Dwellings		P	P	P	P	P
Single Family		P	P	P	N	N
Duplex or Tandem		P	P	P	N	N
Live-Work Unit		P	P	P	P	P
Mobile Home		C	N	N	N	N
Multi-Family		C <sup>(1)</sup>	P <sup>(1)</sup>	P	P	P
Zero Lot Line Single Family		P	P	P	N	N
Zero Lot Line Duplex		P	P	P	C	N
<b>NON-RESIDENTIAL</b>						
Child/Adult Day Care		N	N	C	P	P
Drive-through Facilities	Section G.2.c	N	N	C	C	C
Eating and Drinking		N	N	P	P	P
Hospitals/Clinics		N	N	C	P	P
Hotels/Motels		N	N	P	P	P
Manufacturing/Processing		N	N	C <sup>(2)</sup>	C <sup>(2)</sup>	C <sup>(2)</sup>
Office, Medical Office, Medical/Dental Labs	Section G.2.a	N	P	P	P	P
Parking/Principal Use		N	N	C	C	C
Recreation, Outdoor		N	N	C	C	C
Recreation, Indoor		N	N	C	P	P
Retailing Light		N	N	P	P	P
Retailing Intensive		N	N	N	C	C
Services Personal		N	N	P	P	P
Services Intensive		N	N	N	C <sup>(2)</sup>	C <sup>(2)</sup>
Services Major Vehicle		N	N	N	P <sup>(2)</sup>	P <sup>(2)</sup>
Services Automotive	Sections G.2.b and d	N	N	N	P	P
Warehouse/Showroom		N	N	P <sup>(2)</sup>	P <sup>(2)</sup>	P <sup>(2)</sup>
Wholesale/Warehouse		N	N	P <sup>(2)</sup>	P <sup>(2)</sup>	P <sup>(2)</sup>

P = Permitted; C = Conditional Use; N = Prohibited

<sup>(1)</sup> Limited to 5 units per building

<sup>(2)</sup> Only if the Future Land Use of the site is Industrial.

## 2. USE STANDARDS. (+)

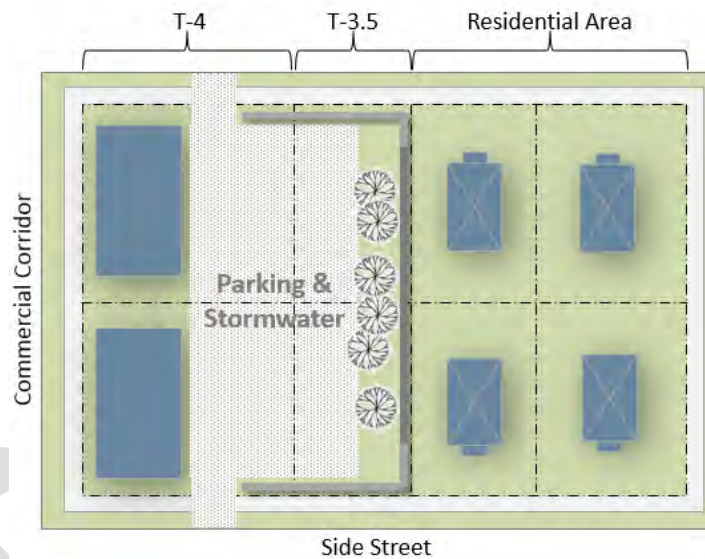
### a. Non-Residential Uses in T-3.5

In order to protect the residential neighborhoods from encroachment of incompatible non-residential uses, the following standards shall apply to non-residential and mixed-uses within the T-3.5 zone:

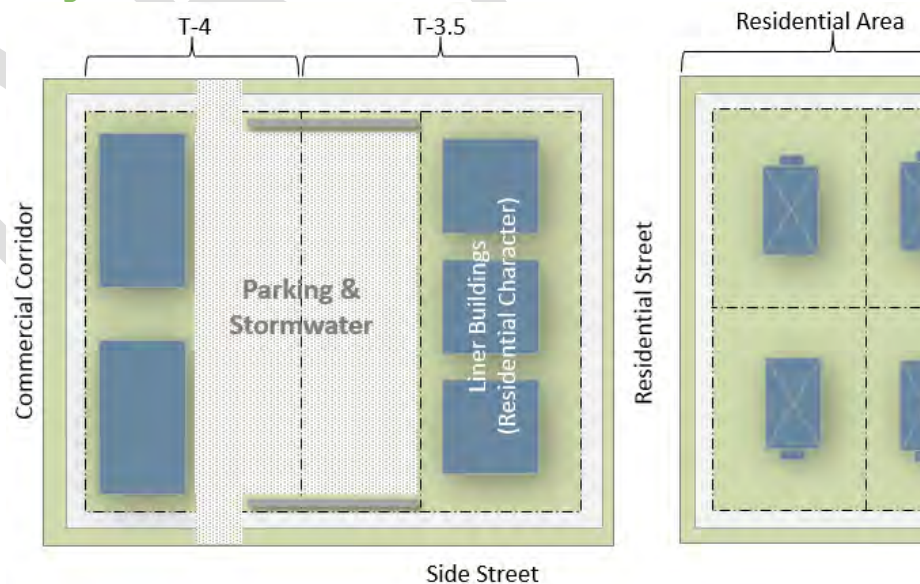
- i. The development shall be rear-loaded and designed to appear residential from adjacent residential streets.
- ii. No new curbcuts shall be allowed along residential streets to serve non-residential developments in T-3.5 zones. The development shall be accessed from adjacent T-4 properties. Cross access easements and unified access and site circulation must be provided to adjacent T-4 and T-3.5 lots unless waived by the Planning Official.
- iii. May allow up to 0.30 FAR of office uses in residential zoning districts if consistent with relevant Growth Management subarea policies.
- iv. Sites in T-3.5 are encouraged to be consolidated with abutting properties facing commercial corridors (particularly S. Orange Avenue, E. Michigan Street, E. Pineloch Avenue, and W. Grant Street) whenever feasible.
- v. The conversion of existing single family homes to office uses shall not be permitted unless the majority of the block face is designated as T-3.5, and a minimum of 3 lots are consolidated prior to the conversion. All access, circulation, parking and stormwater facilities shall also be consolidated and must be provided to the rear of the existing structures. The existing curb-cuts and driveways shall be closed, and fences or walls shall be used to screen all parking areas not already screened by the existing structure. In order to enhance the compatibility of converted offices on Jersey Street with the rest of the residential neighborhood, use of existing homes as offices shall be limited to those office uses that operate between the hours of 7 a.m. and 10 p.m. Special care shall be taken by property owners and tenants of such properties to not disturb the residential characteristics, aesthetics, and general tranquility of the neighborhood.
- vi. Gable, hip or similar sloped roof types are required for any development utilizing more than one story. Flat roofs for office development are prohibited unless approved by the Appearance Review Official. In order to maintain the residential character of these areas, front porches and other traditional residential-style architectural features are encouraged, and may be required during development review to ensure that a residential character is achieved in the building design.
- vii. Stormwater and parking areas may be allowed on T-3.5 lots but only to support adjacent uses as illustrated in Figure 3 when the Zoning Official determines that the consolidated site plan provides appropriate transitions to the adjacent residential neighborhood and meets the following standards:
  - (1) The T-3.5 lot(s) shall be consolidated with a T-4 property as a single development site.
  - (2) A Bufferyard B (8 feet deep with 80% tree coverage and a continuous 6-foot high wall), shall be provided between any parking or vehicular use area and any directly adjacent residential lot.

- (3) Parking areas shall be screened by a street wall (see Section K.3). Groundcover or landscaping shall be provided between the wall and the street.
- viii. Notwithstanding the above, commercial parking or stormwater retention shall not be the principal or only use of T-3.5 lots where the majority of a block face is designated as a T-3.5 transect area. For the purposes of this Plan, "block face" shall be considered to mean all of the lots on one side of a street located between two intersecting streets. Any parking structures, surface parking, or stormwater retention areas developed on T-3.5 lots to serve adjacent T-4 areas must be screened from view from the opposite side of the street with residentially-scaled liner buildings, as illustrated in Figure 4.

**Figure 3. Parking and Stormwater in T-3.5.**



**Figure 4. Parking and Stormwater in T-3.5.**



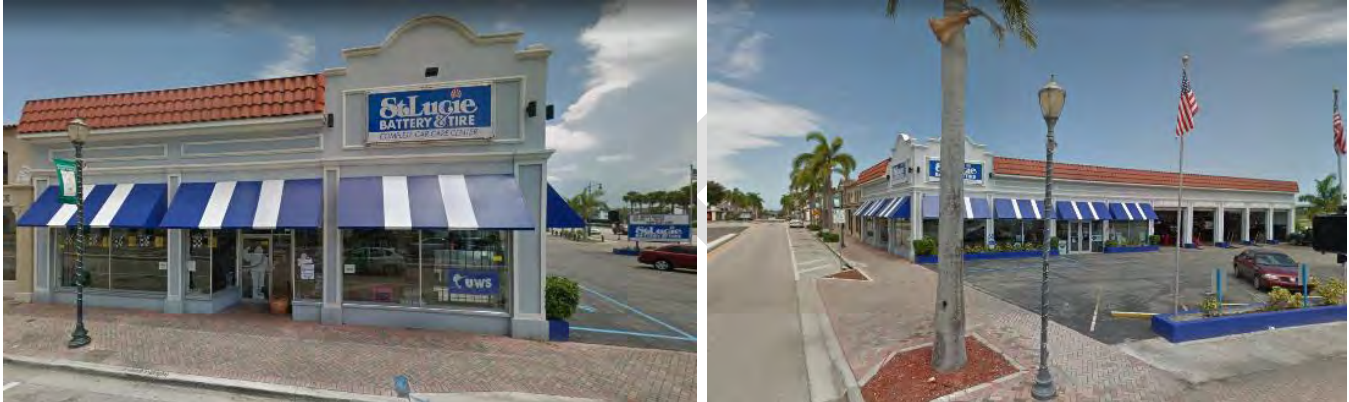


### b. Auto Repair, Carwash (\*)

All principal and accessory structures used for automobile service shall be located and constructed in accordance with the following requirements:

- i. Bay openings shall be located to the side or rear of the building (see Figure 5) and shall be screened from the street.
- ii. All bay openings shall be oriented away from adjacent single family residential districts.
- iii. Accessory car wash structures shall not exceed 20 feet in height.

**Figure 5. Auto Repair Facility**



### c. Drive-through Facilities(\*)

Drive-through facilities are auto-oriented and therefore discouraged from locating within the SODO district. However, one drive-through lane per use may be allowed through the Conditional Use Permit process and subject to the following standards:

- i. Drive-through facilities shall not directly access a Primary Street, as defined in this document and depicted in Figure 74.
- ii. The number of access points to public streets shall be minimized. This may be accomplished through the provision of joint driveway access from the adjacent sites.
- iii. Drive-through lanes and windows shall be located along the side or rear of buildings, away from street frontages (see Figure 6).
- iv. Adequate queuing space shall be provided for drive-through windows and order stations in compliance with the City code.
- v. Drive through facilities integrated into structured parking (see Figure 7) may be approved by the SODO TDR instead of CUP.

**Figure 6. Exterior Drive-Through Facilities**



*Examples of appropriate design for drive-through facilities (building up to the street; drive-through window in the rear). Note that the streetscape area in front of the building does not meet the standards of the district.*

**Figure 7. Interior Drive-Through Facilities**





**d. Service Stations (\*)**

Service stations are auto-oriented uses and may be permitted as shown in Figure 2 of the LDC or Table 1 (Alternative Uses). However, all gas stations shall meet the standards of this document and the following provisions:

- i. The convenience store or building shall be located in the front of the site (see Figure 8) meeting the required building setback of the transect zone. The pumps may be located to the side or rear of the main building.
- ii. A street wall meeting the standards of Section K.3 shall be provided to screen vehicular use areas. Landscaping (up to 3 feet in height) shall be provided in front of the wall to soften the treatment.
- iii. Auto repair bay and car wash openings, service and storage areas, and refuse enclosures shall be oriented away from public view.
- iv. Gas station canopy clearance shall not exceed eighteen (18) feet.
- v. Gas station canopy roofs may not be flat and should incorporate some of the architectural features used in the main building, such as decorative columns, roofing forms and materials. (+)
- vi. Lighted bands or tubes or applied bands of color (other than permitted as signage) are prohibited. (+)
- vii. Site lighting should minimize direct and reflected glare and excess brightness. Therefore, only cut-off fixtures shall be allowed consistent with the City lighting code.

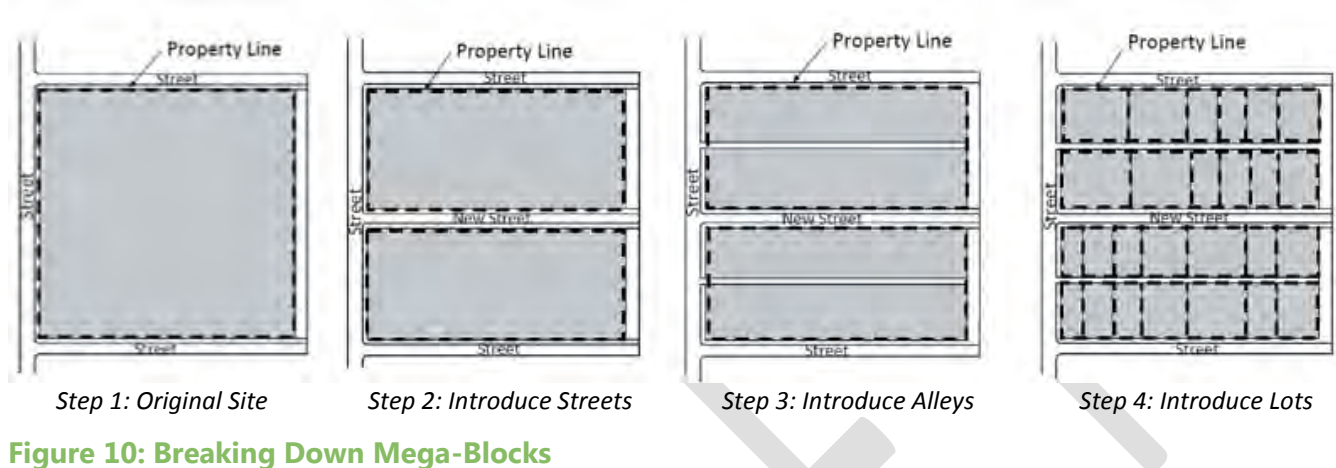
**Figure 8. Service Stations**



## H. BLOCK & LOT STANDARDS (\*)

The regulation of block size is a fundamental component of a code to achieve good urban form and transportation connectivity. Shorter blocks improve the pedestrian experience as well as foster a street network grid that supports the efficient distribution of automobile traffic.

**Figure 9. Creating Connectivity**



**Figure 10: Breaking Down Mega-Blocks**



*Suburban Mega-Blocks vs. Urban-Scaled Blocks [Source: Lansing Master Plan]*



*Former mega-block, divided into smaller blocks (SoDo)*

The SODO district generally displays a gridded network, which shall be maintained and improved. If existing streets are vacated to allow the aggregation of smaller blocks into mega-blocks, alternative vehicular connections shall be provided. Furthermore, proposed developments shall be required to increase connectivity in the area by meeting the following standards:



1. Culs-de-sac and dead-end streets are not allowed, unless physical conditions provide no practical alternative. (\*)
2. New blocks shall not exceed a maximum length of 660 feet.
3. Blocks do not have to be orthogonal, but streets shall be interconnected.
4. Existing development sites that do not encompass an entire block but are within a block that exceeds the maximum block perimeter allowed, shall incorporate vehicular passages to rear or side streets, or to adjacent sites (for future connection if not feasible at the time) to ensure the 660-foot maximum length is not exceeded (see Figure 11). The vehicular passage may be a public street or private drive but shall be open for public passage to another street/drive, or to the adjacent site, which will be required to continue that street/drive.
5. Pedestrian passages to rear parking areas or streets, or to adjacent sites (for future connection) shall be provided every 300 feet, where applicable (see Figure 11). Where there is not a functional connection, a public plaza with a minimum width and depth of twenty (20) feet shall be created to break down the continuity of the block.

**Figure 11: Examples of Vehicular and Pedestrian Passages**



*Vehicular Connection*



*Pedestrian Passage*

## I. SITE AND BUILDING DESIGN STANDARDS (\*)

### 1. BUILDING SETBACKS AND IMPERVIOUS SURFACE.

The placement of a building on a site is critical to creating a coherent public realm. Urban development forms seek to support and foster pedestrian trips. Therefore, buildings need to be constructed in closer proximity to streets and sidewalks but not so close that the public realm is diminished. The intent of the SODO building setback standards is to shape the public realm, and strengthen the physical and functional character of the area. Figure 12 shows the location of each of the required building setbacks.

**Figure 12: Building Setbacks**

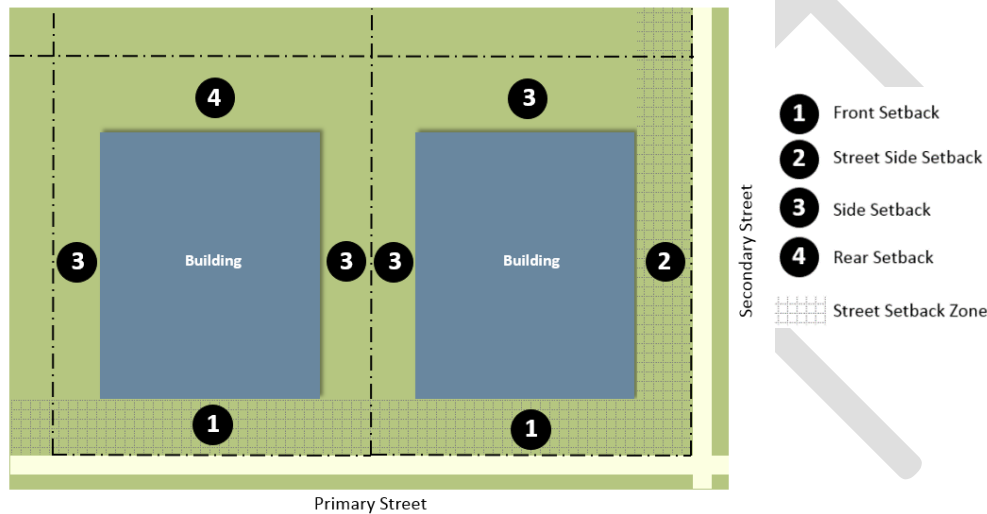
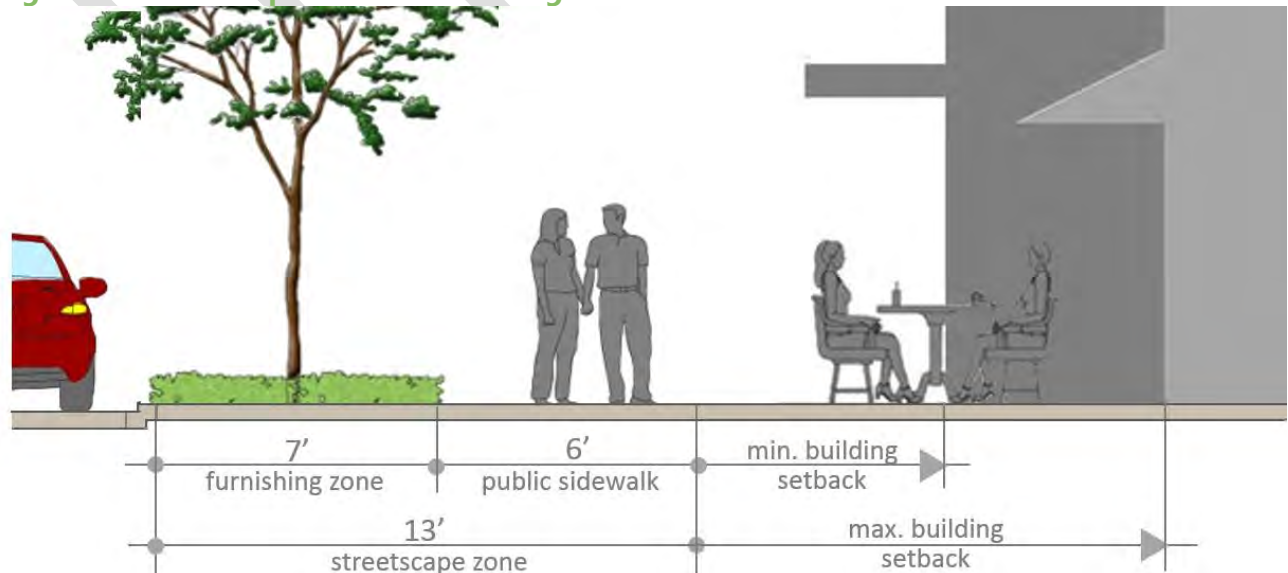


Table 2 lists the required building setbacks and the maximum impervious surface area allowed. The building setbacks along a street shall be measured after allocating enough room to accommodate a 13-foot streetscape zone (7-foot wide furnishing zone adjacent to the curb, and a 6-foot wide public sidewalk between the furnishing zone and the site). Should any part of the streetscape zone fall within private property an easement will be required over that area. See Figure 13.

**Figure 13: Streetscape Zone and Building Setback**



- a. In order to encourage variety in building setbacks along the corridor, a maximum of 30 percent of the primary building facade may be permitted to encroach up to 7 feet into the required front and/or street side setback, but not into the streetscape zone.
- b. For attached dwellings and townhouses, the minimum side yard setback noted in Table 2 applies to the end units of each building.
- c. Whenever a new building is being built adjacent to a building that has a zero-foot sideyard setback, and a zero foot setback is allowed by the T-zone, the new building shall either have a zero-foot setback or provide a minimum side yard setback of seven and one-half (7.5) feet.
- d. There is a seven and one-half (7.5) foot minimum building setback required from the railroad right-of-way.

**Table 2. Building Setback Requirements**

	T-3	T-3.5	T-4	T-5	T-6
<b>① Front and ② Street Side</b>	15' min.	15' min.	7 min. 15 max.	7 min. 15 max.	7 min. 15 max.
<b>③ Side</b> (See Section. I.5, Site Setback/ Building Frontage)	5' min.	5' min.	0' min.; 20' min. adjacent to single- family zoning. Max: See Section. I.5.	0' min. Max: See Section. I.5.	0' min. Max: See Section. I.5.
<b>④ Rear</b>	15' min.	15' min.	10' min.; 20' min adjacent to single- family zoning. With Alley: 3' min.	10' min. With Alley: 3' min.	10' min. With Alley: 3' min.
<b>Step Back Next to Single Family Zoning</b> (Section I.9)	N/A	N/A	10' additional setback for each additional floor above 2.	10' for each additional floor above 2.	N/A
<b>Impervious Surface Ratio (max.)</b>	0.6	0.6	0.9	0.9	0.9

Table Notes:

Front and Street Side Setbacks are measured from the back of the Streetscape Zone.

The building frontage treatments listed in Figure 40 may project up to eight (8) feet into the street setback, but not into the public right-of-way.

## 2. STREET SETBACK ZONE DESIGN. (\*)

The intent of the required Street Setback Zone (front and street side setbacks) is to provide a transition, both physical and visual, from the street to the building. The zone created by the setback should vary in design depending on the level of privacy desired along the building façade. Commercial buildings usually have a setback zone designed to attract customers into the building, while residential, hotel and office buildings often have a setback zone designed to provide privacy to the ground floor spaces, as shown on Figures 14, 15 and 16. Handicap accessibility laws shall be observed.

- a. Street Setback Zones in front of uses that benefit from pedestrian interaction along the front façade may include urban landscaping such as containers and/or planter boxes that complement the building mass and architecture.
- b. Street Setback Zones in front of uses that do not require pedestrian interaction along the façade (e.g. offices, hotels, multifamily) may be landscaped with a combination of intermediate (understory) trees, palms, shrubs, vines and/or ground covers.
- c. Street furniture such as benches, trash receptacles, and/or bicycle racks may be installed within the Street Setback Zone.
- d. Outdoor dining is permitted within street setback zones as long as restaurants are a permitted use.
- e. Elements within the Street Setback Zone (landscaping and architectural features) shall comply with the vision triangle requirements established using AASHTO standards.
- f. The proposed building ground floor along a Primary Street (see Figure 74) shall contain active uses oriented to the street. Active uses may include display or floor areas for retail uses, waiting and seating areas for restaurants, atriums or lobbies for offices, and lobbies or dining areas for hotels or multi-family residential buildings.
- g. Cantilevered balconies, bay windows, roof overhangs and similar elements may encroach into the Street Setback Zone but not the public right-of-way (see Section I.14 and Figure 41).
- h. The Street Setback Zone may be used to accommodate grade changes necessary to direct stormwater to the back of the site, and /or necessary to access the building which may be at a different grade than the public sidewalk. Grading may be achieved with landscaping, steps, elevated platforms, stairs, or other treatment.



*Street Setback Zone used to accommodate steps*



**Figure 14: Example of Street Setback Zone Used for Outdoor Seating Area***(graphic not to scale)***Figure 15: Example of Street Setback Zone Used for Expanded Pedestrian Area***(graphic not to scale)*



**Figure 16: Examples of Street Setback Zone Used for Landscaping/Privacy***(graphic not to scale)*

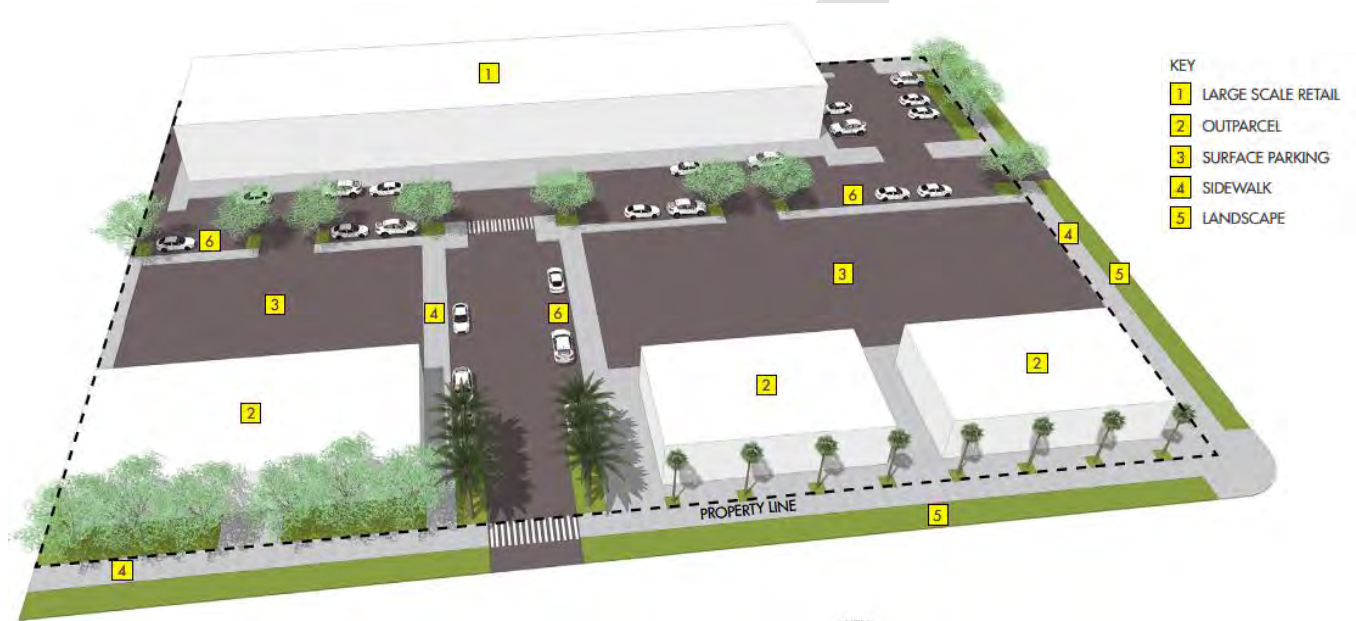
\* See SODO Complete Streets Master Plan for landscape/furniture strip and sidewalk design.

### 3. OUTPARCELS

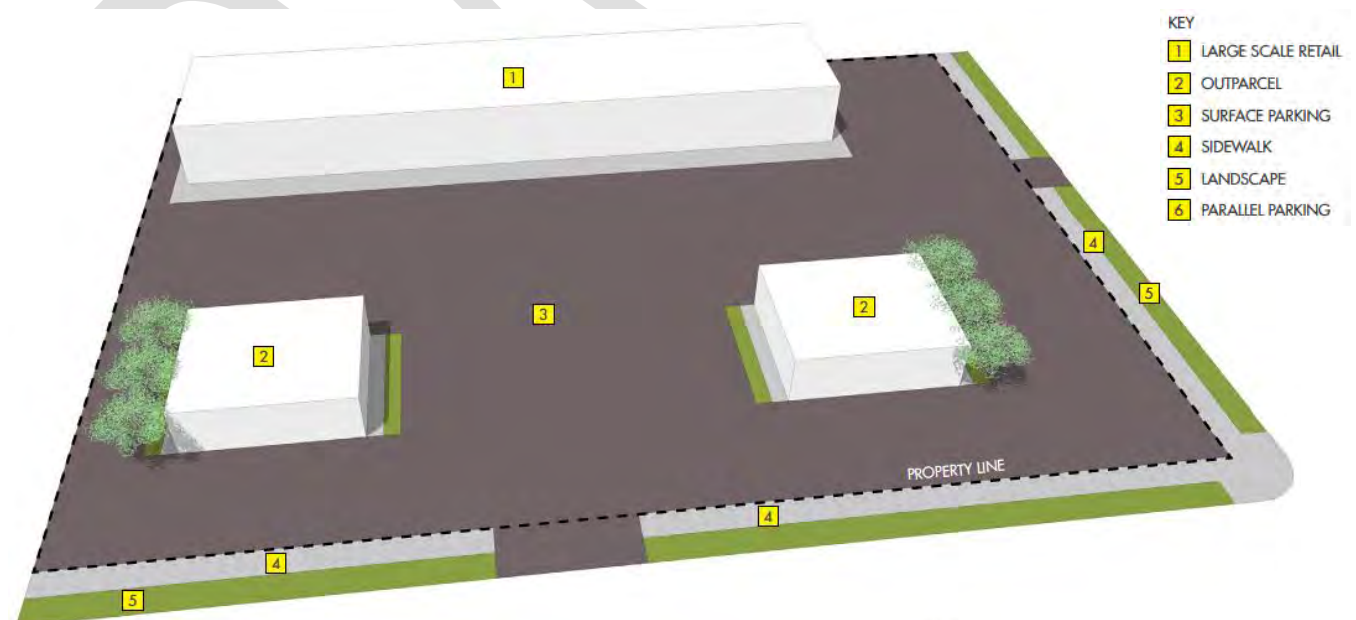
The placement of some buildings at the rear of a site is permitted as long as one or more buildings are placed in front to meet the requirements of this code (see Figure 17 for an acceptable design alternative). In such cases, access drives shall be incorporated into the site layout to create connectivity to other sites and streets. The main access drive shall be centered on the anchor building and shall be lined with buildings or sidewalks and urban landscaping.

**Figure 17: Multiple Buildings on a Site**

*Recommended:*



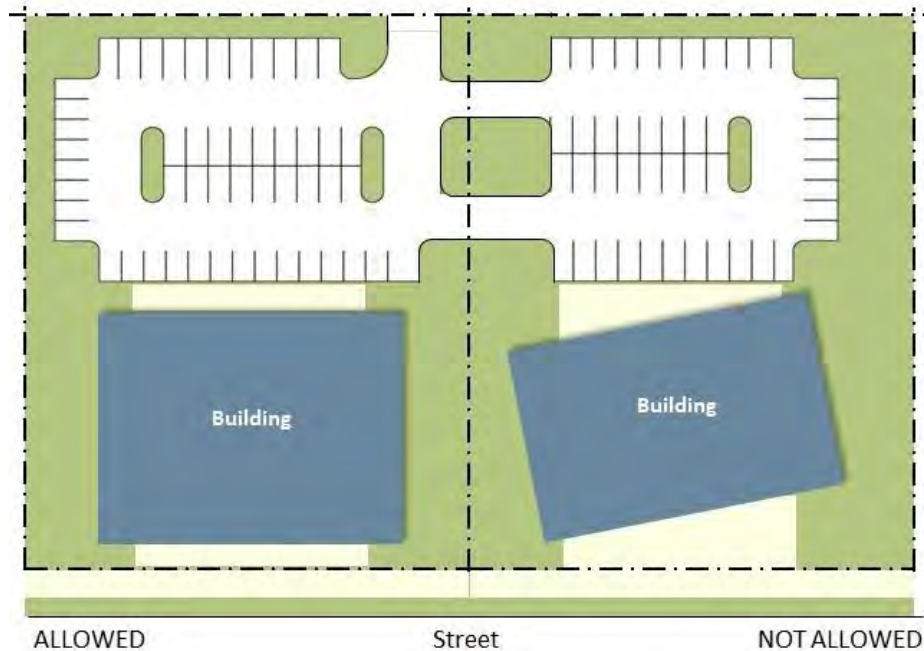
*Prohibited:*



#### 4. BUILDING ALIGNMENT. (+)

Building facades facing a right of way shall be built predominantly orthogonal (parallel) to the street, as shown on Figure 18.

**Figure 18: Building Alignment**

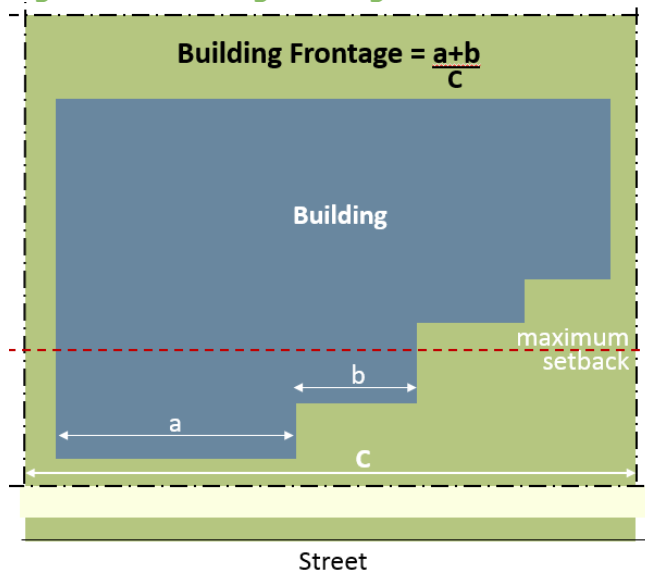


#### 5. SIDE SETBACK/BUILDING FRONTAGE. (+)

The purpose of the following building frontage requirements is to ensure façade continuity and activity along the street, in addition to avoiding large expanses of blocks that are not framed by buildings. The building frontage standards are stated as a proportion of the building width (within the required building setback) relative to the width of the development site measured at the site frontage line. Portions of the building façade outside the required building setbacks do not count as building frontage (see Figure 19). The standards of this section do not apply to single family homes and duplex units.

- The minimum required building frontage varies based on the transect zone, building type and street type. The requirements are as follows:

**Figure 19. Building Frontage Calculation**





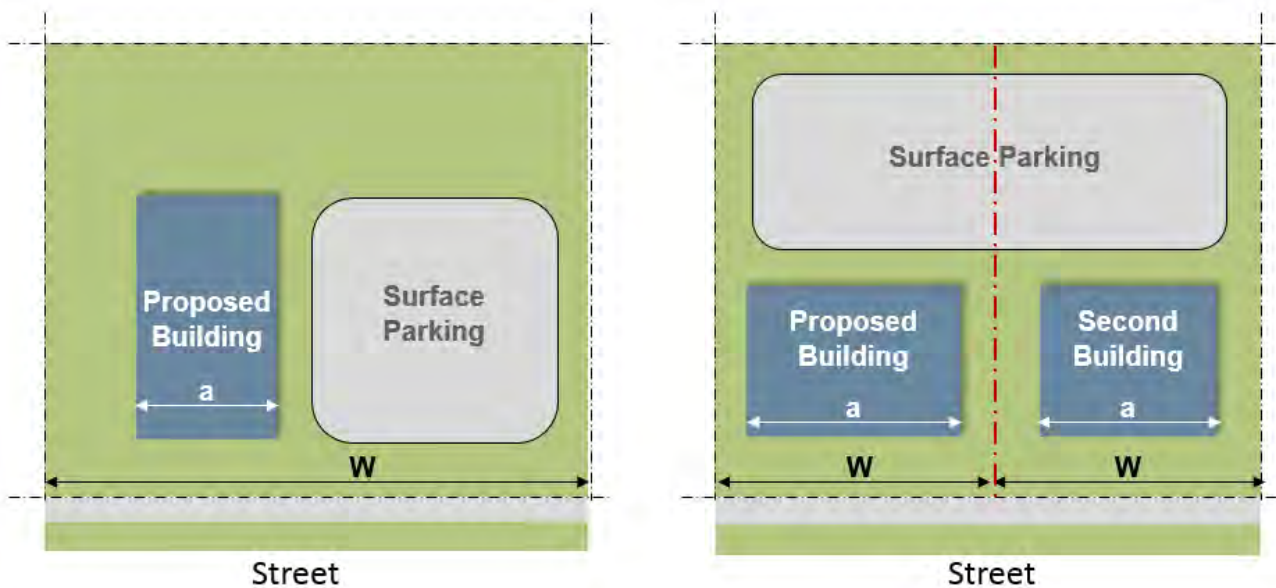
**Table 3. Building Frontage Requirements**

	T-3	T-4	T-5	T-6
<b>Primary Street</b>				
Mixed-Use, Single Use	NA	65%	70%	70%
Live-Work, Single Family	40%	NA	NA	NA
Civic, Industrial	40%	40%	40%	40%
<b>Secondary Streets</b>				
Mixed-Use, Single Use	NA	50%	50%	50%
Live-Work, Single Family	20%	NA	NA	NA
Civic, Industrial	40%	40%	40%	40%

See Section B, Definitions, and Figure 74 for Street Types.

b. Exceptions to the Building Frontage requirements:

- i. In the event the proposed building width is too narrow to meet the minimum frontage requirement, the applicant shall have the option of dividing the lot into smaller, narrower lots to meet the dimension requirements, as shown on Figure 20. No platting will be required, but the concept plan shall show the new lots as available for future development.

**Figure 20. Exceptions to the Building Frontage**

Left diagram: Building width ( $a$ ) is too narrow to meet the minimum building frontage.

Right diagram: Subdivide the lot to decrease its width ( $W$ ) to meet the minimum building frontage.

- ii. In the case where the required building frontage cannot be met due to the need to provide vehicular access from the street, a gateway, arch, or similar feature shall be provided to preserve the block continuity and may be counted toward meeting the building frontage requirement, as shown on Figure 21.

**Figure 21. Gateway Feature***Gateway feature designed to meet minimum building frontage.*

- iii. Single family, duplex, townhomes, libraries, places of religious assembly, public utility buildings, and schools (elementary, middle and high) are not subject to the minimum frontage requirements.
- iv. Drive-through facilities proposed as part of a development subject to the requirements of this section must be designed to comply with the building frontage requirements.
- c. No maximum lot width is prescribed for development within the SODO district. However, the width of a lot shall not be justification for not meeting the building frontage requirements.

## 6. ARCHITECTURAL STYLE.

While there is not a requirement for a specific architectural style to be used, buildings should adopt an identifiable architectural style, but avoid becoming replicas of historical buildings. The incorporation of traditional elements into a modern style and asymmetrical facades are acceptable alternatives to traditional styles. Additionally, all buildings shall meet the standards of this section to ensure compatibility in massing, scale and interaction with the public realm. Parking garages are an integral part of an urban area and their design can have a major impact on the appearance of an area. Therefore, parking garages are subject to the same building design requirements as all other buildings.

## 7. GREEN BUILDING.

Green building principles related to energy efficiency, resource protection and environmental protection (e.g. reusable building materials, light colored roof materials, living roofs to treat stormwater, energy efficient windows, solar energy, etc.) set forth by agencies such as, but not limited to, the United States Green Building Council, the Florida Green Building Coalition and similar agencies are encouraged.

## 8. BUILDING TYPOLOGY.

Form-based regulations use physical form, rather than separation of land uses, as their organizing principle of a community. Buildings within the SODO district shall adopt one of the following building typologies based on the location of the property within one of the transect zones. The typologies do not necessarily refer to the use of the building, but rather to character of it. For instance, the Single-

Family typology may house a duplex, multi-family or even an office, if the use is allowed in the district/t-zone.

- Mixed Use – T-4, T-5 and T-6
- Single Use Non-Residential – T-4, T-5 and T-6
- Single Use Multi-Family – T-4, T-5 and T-6
- Live-Work – T-3.5
- Single-Family – T-3.5, T-3
- Civic – All transect Zones

All development (residential and non-residential) in T-3.5 shall utilize the Single-Family building typology in terms of roof forms, scale, massing and building materials historically found within the Traditional City.

#### a. Mixed-Use Building

A type of building designed for ground floor, street frontage occupancy by retail, service, and/or office uses, with upper floors configured for office use or for dwelling units.

- i. The ground floor shall be occupied by retail, service, and/or office uses; the upper floors may include non-residential use and/or residential dwelling units (see Figure 22).
- ii. The ground floor must be designed for maximum pedestrian interaction (shop fronts, outdoor cafes, etc.).
- iii. Mixed-Use buildings may incorporate residential units in a live-work format.

**Figure 22. Mixed Use Buildings**







### b. Single Use Non-Residential Buildings

A type of building designed to accommodate a stand-alone non-residential use. Examples include banks, hotels, restaurants, and office buildings.

- i. Buildings on corner lots shall be designed with two façades of equal architectural expression.
- ii. Large scale buildings such as big box retailers, movie theaters, wholesale stores, shall meet all site and building design requirements of this code.
- iii. Auto repair, drive-through and service station uses are only permitted as noted in Section G.2 of this document.
- iv. The main entrance to the building shall face the primary street and be designed to stand out.



### Figure 23. Single Use Buildings

Examples of appropriate design for supermarkets:



Examples of other single-use buildings:





### c. Single Use Multi-Family (\*)

A type of building designed to accommodate a townhouse, condominium or apartment use (see Figure 24). May range in height from 2 to several stories if allowed in the district/t-zone.

- i. Buildings shall be positioned along the street to meet the setback and building frontage standards (see Sections I.1 and I.5).
- ii. In the case of single entrance buildings, the entrance to the buildings shall face the right-of-way and shall be designed to stand out (see Figure 25). Buildings internal to the site may have their entry facing a central landscaped courtyard or parking area.
- iii. Buildings with multiple entrances (e.g. townhouses) shall have entrances along the façade facing the right-of-way (see Figure 26).
- iv. Balconies are encouraged.
- v. Multifamily developments shall be designed to promote pedestrian and bicycle circulation within the development and to promote access to surrounding areas.
- vi. Parking garages shall not dominate the site frontage, and shall, if feasible, be accessed from an alley or side street. If access must be provided from the principal street, the design of the building shall minimize the impact of such access by providing a covered entrance (bridge or tunnel – see Figure 27)
- vii. Multi-family developments are encouraged to provide electric vehicle charging stations to eliminate the possibility of extension cords stretching from residences into parking areas.



Figure 24. Multi-Family Examples



Figure 25. Prominent Single Entrances





**Figure 26. Multiple Entrances****Figure 27. Vehicular Access to Internal Parking**

#### **d. Live-Work Buildings**

A type of building designed to accommodate a residential use with a business component.

- i. Live-work units shall be predominantly residential. Business uses may be allowed (if permitted or conditional use in the zoning district) on the ground floor facing the primary street, with the residential use on upper levels or behind the non-residential use (see Figure 28).
- ii. The non-residential space is subject to applicable building codes, zoning and BTR (Business Tax Receipt).
- iii. The non-residential space on the first floor shall be oriented toward the street.
- iv. Building access shall be provided in the form of single or separate entrances for the non-residential area and residential occupancy areas.

- v. The building shall have a residential character and shall meet the building placement and frontage requirements of Sections I.1 and I.5.
- vi. Flat roofs are not allowed on Live-Work buildings east of Orange Avenue, unless they are combined with, and secondary to, sloped roofs.
- vii. For these units to operate as a live-work unit, they shall be owner-occupied.
- viii. No additional parking spaces are required to serve the non-residential portion.

**Figure 28. Live-Work Units**



#### **e. Single Family Building**

A type of building designed as a single dwelling, or to resemble one, with either an attached or detached garage. The T-3.5 transect zone allows multi-family and office uses if designed utilizing this building typology (see Figure 29). The Single-Family Building shall be designed as noted below:

- i. A porch or stoop facing the street shall be provided.
- ii. Flat roofs are not allowed as the principal roof structure, unless they are combined with, and secondary to, sloped roofs.
- iii. Garage frontage shall only be allowed for buildings that are actually used as a single-family home and shall not comprise more than fifty (50) percent of the building's front façade in accordance with Traditional City standards.
- iv. Garages or carports shall be provided in one of two ways:
  - o Attached and recessed from the primary façade (not including porches, bays, or other minor projections) by a minimum of five (5) feet, or
  - o Placed in the rear yard and accessed by either an alley or a side yard driveway.



Figure 29. Single Family Building Examples



*Office Use in Single Family Building*

*Multi-family use*



## f. Civic Buildings

A type of building designed to accommodate public or institutional uses such as a civic center, fire or police station, museum, government building, postal services, library, or school (see Figure 30).

- i. Civic buildings should be placed in central locations as highly-visible focal points. They shall not be located within storefronts or shopping centers.
- ii. Where feasible, civic buildings should be close to transit stops for ease of use by pedestrians.
- iii. The building may be setback from the street to accommodate a public gathering space in the front yard.
- iv. Drop off areas shall be placed in the rear of the building.
- v. Symmetry is encouraged in the building design.

**Figure 30. Examples of Civic Buildings**

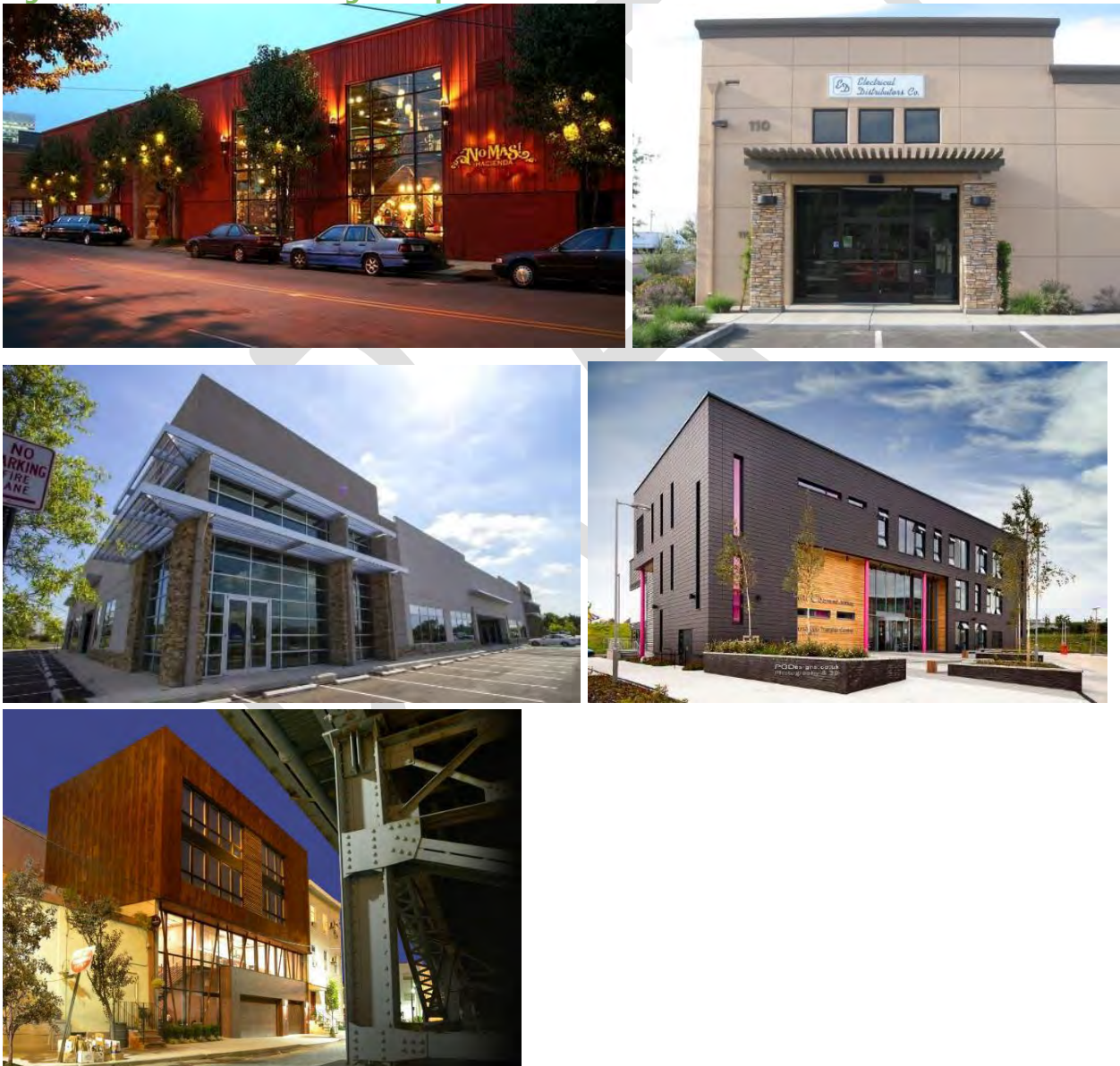


### g. Industrial Building (\*)

Certain zones in SODO allow industrial use interspersed with commercial and residential uses. The Industrial Building typology standards are intended to ensure compatibility between these typically incompatible uses (see Figure 31).

- i. Industrial buildings are allowed more flexible building frontage and fenestration standards than mixed-use and commercial uses, but are still required to provide quality design and respect the pedestrian scale of the district along the public street. Therefore, industrial development shall meet the standards of this Code.
- ii. The office portion of the industrial site shall be located along the site frontage, and the more intensive industrial operations shall be located in the rear.
- iii. Buffering and screening per LDC Chapter 60, Part 2E shall be provided.

**Figure 31. Industrial Building Examples**



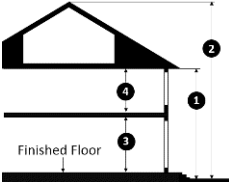
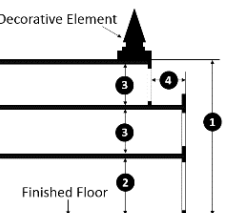
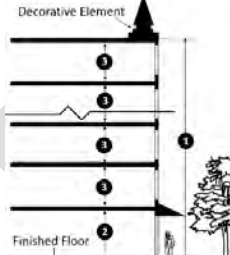
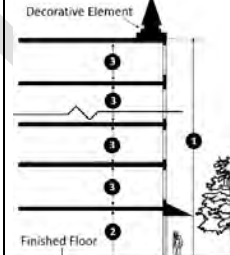


## 9. BUILDING HEIGHT. (\*)

*Building height shall be measured in stories and shall meet the standards provided in Table 4.*

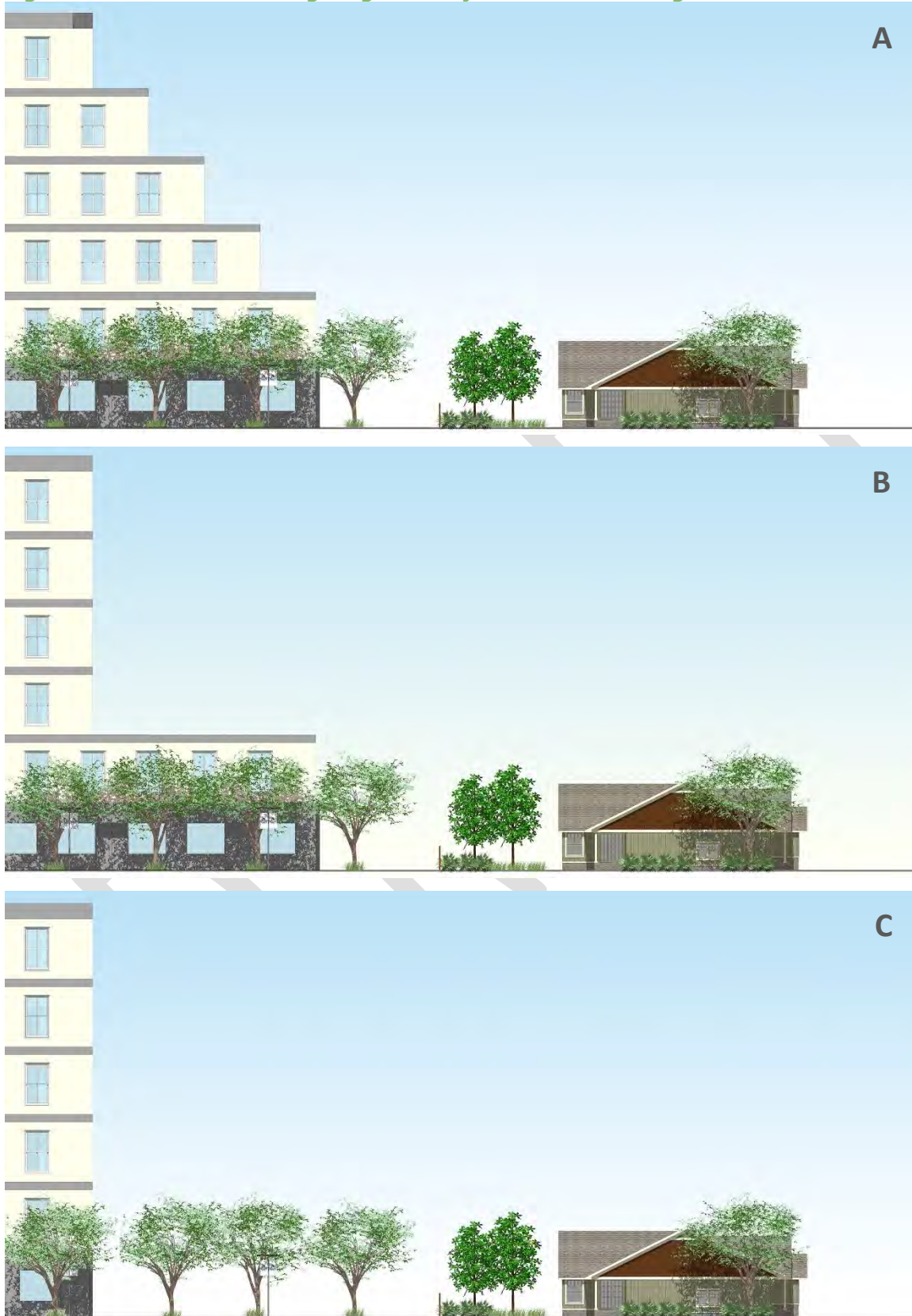
- Decorative elements such as spires, minarets, clock towers, and cupolas are allowed over the maximum number of stories as long as they do not exceed 20% of the proposed building height.
- An additional building setback of ten (10) feet over the minimum required for each floor above two (2) stories in T-4 and T-5 zones, shall apply to buildings facing a single family residentially-zoned or T-3 and T-3.5 property. Developers may elect to apply the setback just to those floors above the second story (step back approach, see Figure 32 - Option A), to the entire façade (Option B) or a combination of the two (Option C). The additional setback/step-back requirement may render some sites ineligible for the maximum permitted height.

**Table 4. Building and Floor Height**

	T-3 & T-3.5	T-4	T-5	T-6
				
<b>Building Height:</b>				
<b>Min.</b>	NA	20' overall	20' overall	20' overall
<b>Max.</b>	2.5 stories 24' to eave ① 35' overall ②	3 stories ①	5 stories ①	9 stories ① (120 ft. max.)
<b>Height Bonus (CUP)<sup>(1)</sup></b>	Not allowed	Up to 4 stories (south of Michigan Avenue only)	Not allowed	Up to 17 stories (200 ft. max.)
<b>Floor Height</b>				
<b>Ground Floor (min.)</b>	8' for residential/ 12' for non-residential use ③	10' for residential/ 12' for non-residential use ②	10' for residential/ 12' for non-residential use ②	10' for residential/ 12' for non-residential use ②
<b>Upper Floors (min.)</b>	8' ④	8' ③	8' ③	8' ③
<b>Ground Floor Elevation</b>	18" min.	6" max.	6" max.	6" max.
<b>Step Back Next to Single Family Zoning (*)</b>	N/A	10' for each additional floor above 2. ④	10' for each additional floor above 2. ④	N/A

<sup>(1)</sup> See Section N, Development Bonuses.

Figure 32. Setbacks Abutting Single-Family Residential Zoning



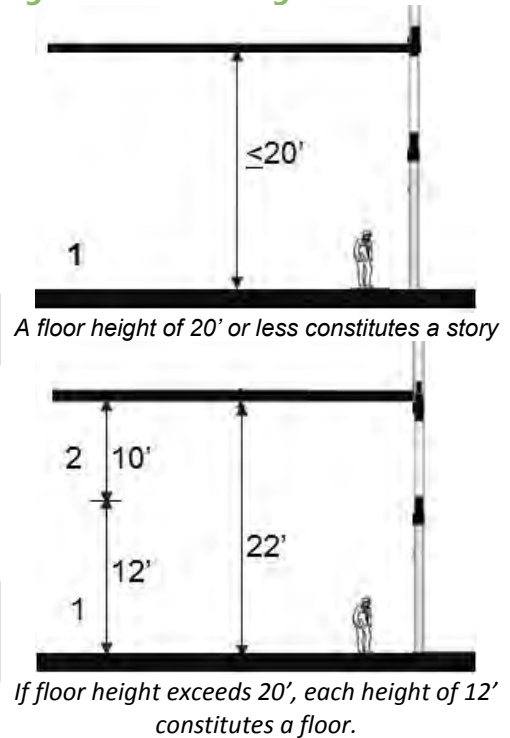


## 10. FLOOR HEIGHT. (\*)

Floor height shall be measured as the clear height from finished floor to finished ceiling (see Table 5).

1. Whenever a ground floor level exceeds twenty (20) feet in height, each height of twelve (12) feet or portion thereof shall be construed to be one (1) story (see Figure 33).
2. Whenever a floor other than a ground floor exceeds twelve (12) feet in height, each height of twelve (12) feet or portion thereof shall be construed to be one (1) story.
3. Mezzanines extending beyond 33% of the floor area shall be counted as an additional story.
4. Parking garages are exempt from the floor height requirements.
5. The first floor of residential buildings shall be elevated above the finished grade adjacent to the building. The building should incorporate either a raised concrete pad or a raised wood joist floor with perimeter foundation at a minimum of eighteen (18) inches above the finished grade.

**Figure 33. Floor Height**



## 11. BUILDING MASSING. (\*)

Buildings shall respect and reflect pedestrian scale by using the following techniques:

- a. Buildings shall be articulated as shown in Table 5. The purpose of this requirement is to make buildings appear as multiple smaller volumes grouped together. This can be achieved through variation of building height and width dividing volumes into distinct massing elements. In no event shall buildings exceed a height to width ratio of 1:3 or the length noted in the table, whichever is less, without providing a substantial volume break, which may consist of a projection or recess, a tower or bay, and/or an architecturally prominent entrance (see Figures 34 and 35). These vertical and horizontal projections and recesses shall have a minimum depth as noted in Table 5.

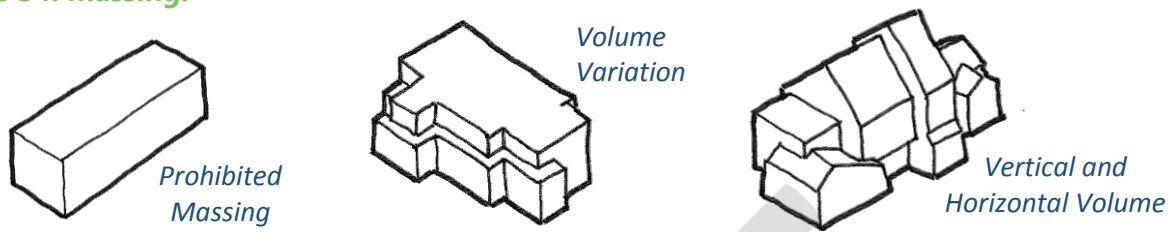
**Table 5: Building Massing (\*)**

	T-3 & T-3.5	T-4	T-5	T-6
Building Length	75' max.	NA	NA	NA
Articulation Spacing	Every 30'	Every 60'	Every 120'	Every 120'
Projection/Recess Depth	2'	3'	3'	3'

- b. Roofs or assemblies of roofs may also be articulated to reduce building mass. Roof heights shall vary using the parameters listed in subsection a, above.

- c. Buildings on corner lots and buildings that terminate vistas shall incorporate distinctive architectural treatments to emphasize their prominent location.

**Figure 34. Massing:**



**Figure 35. Example of Building and Roof Articulation**



## 12. FAÇADE ARTICULATION. (\*)

The standards contained in this subsection apply to all buildings and structures, including parking garages. The elements that make up a building façade are key components for defining the public realm. The façade design standards contained in this subsection are not intended to regulate style or appeal. The purpose of these standards is to ensure facades are designed to:

- Reduce the uniform monolithic appearance of large unadorned walls by requiring architectural detail;

- In the case of commercial buildings, ensure the building facades are inviting; and,
- Increase public safety by designing buildings that provide human surveillance of the street.

Building facades along public or private streets shall maintain a pedestrian scale and integrate the public and private spaces using architectural elements as follows:

- a. Non-residential, mixed-use and multi-family buildings shall be required to incorporate into their facades a minimum of three (3) design treatments from the following list (see Figure 36):
  - i. Any of the building frontage treatments listed in Figure 41 integrated with the building mass and style;
  - ii. Overhangs of no less than three (3) feet in depth;
  - iii. Raised cornice/parapets over doors;
  - iv. Expression line between floors;
  - v. An offset, column, reveal, void, projecting rib, band, cornice, or similar element with a minimum depth of six (6) inches;
  - vi. Peaked roof forms;
  - vii. Clock or bell towers;
  - viii. Balconies;
  - ix. Awnings, canopies or marquees;
  - x. Windows or doors;
  - xi. Any other treatment that meets the intent of this section and is approved during the review of the concept plan.
- b. Façades shall not exceed twenty (20) horizontal feet and ten (10) vertical feet without including at least one (1) of the elements mentioned in subsection a, above.
- c. Architectural treatments on the façade, such as cornices or expression lines, shall be continued around the sides of the building.
- d. All exterior facades of outparcel buildings shall be considered primary facades and shall employ architectural embellishment and landscape design treatments on all sides.

**Figure 36. Examples of Façade Elements**



**Figure 37. Examples of Façade Articulation**





### 13. BUILDING ENTRANCES.

- a. The *main* entrance of *all* buildings shall be oriented toward the public right-of-way (see Figures 38 and 39).
- b. Where parking areas are located behind the building, a secondary entrance may be provided from the parking area into the building. Awnings, landscaping, and appropriate signage may be used to mark the secondary entrance.
- c. Entrances on the front façade shall be operable, clearly-defined and highly-visible. In order to emphasize entrances, they shall be accented by a change in materials around the door, recessed into the façade (alcove), or accented by an overhang, awning, marquee or similar feature.
- d. Pedestrian connections from the public sidewalk and parking areas to the building entrance shall be provided.
- e. Residential building entrances shall be designed with porches or stoops.

**Figure 38. Example of Well-Defined Entrance**



**Figure 39. Corner Entrance**



### 14. BUILDING FRONTAGE DESIGN.

All buildings shall utilize one of the following frontage designs and shall meet the standards shown in Figure 41. Awnings, marquees, galleries, arcades, stoops and porches may encroach into the Street Setback Zone but not the public sidewalk, unless specifically stated below or in Figure 41.

- a. **Storefronts.** Storefronts are façades placed at or close to the setback line, with the entrance at sidewalk grade. They are conventional for retail uses and are commonly equipped with cantilevered or suspended canopies, or awning(s).
  - i. Doors shall not be recessed more than five (5) feet from the storefront façade.
  - ii. The storefront windows shall not have opaque or reflective glass. Where privacy is desired for uses such as restaurants and professional services, windows may be divided into smaller panes.
  - iii. Clerestory windows are horizontal panels of glass above the storefront door and display windows. They are a traditional element of "main street" buildings, and are recommended for storefronts.
  - iv. Doors should be well defined and shall match the materials, design, and character of the display window framing.

- v. A cornice or recess shall be provided at the second floor (or roofline for a one-story building) to differentiate the base of the building from upper levels and to add visual interest.
- b. **Awnings/Marquees.** Awnings and marquees may be used to accent windows and doors and to protect pedestrians from the elements.
  - i. High-gloss materials or fabrics that resemble plastic and aluminum shall not be permitted materials for awnings.
  - ii. The highest point of a first floor awning on a multi-story building shall not be higher than the midpoint between the top of the first story window and the bottom of the second story window sill (see Figure 40).
  - iii. Awnings should not cover architectural elements such as cornices or ornamental features.
  - iv. Awnings should be sized to match the corresponding window openings. Their shapes, materials, proportions, design, color, lettering, and hardware also need to be in character with the style of the building.
  - v. Awnings, awning frames, marquees and overhangs may extend over the right-of-way but shall provide a minimum vertical clearance of nine (9) feet above the sidewalk at their lowest point and shall not project closer than twenty-four inches from the vertical projection of the back of the street curb line.

**Figure 40: Awnings**



- c. **Galleries and Arcades.** An arcade is a type of frontage where a cantilevered shed or a colonnade is placed in front of the building to provide protection from sun and inclement weather. Galleries are façades with an attached colonnade that may extend above the ground floor.
  - i. Galleries and arcades are encouraged along pedestrian commercial corridors.
  - ii. Along storefront streets, the arcade/gallery openings shall correspond to storefront entrances.
  - iii. Galleries may be one (1) or two (2) stories, typically with a minimum depth of ten (10) feet.
  - iv. Arcades and galleries must have consistent depth along a frontage.

- v. Floors above an arcade or gallery shall also be allowed to encroach into the street setback zone for the entire depth of the arcade or gallery.
- d. **Forecourts.** Forecourts are uncovered courtyards within a frontage wherein a portion of the façade is recessed from the building frontage. The courtyard is suitable for outdoor activities such as dining or passive recreation.
  - i. Buildings with frontage at the intersection of two Primary Streets (as defined in this document) are required to provide a forecourt at the corner, unless the Appearance Review Official approves an alternative design.
  - ii. Forecourts are not intended to be covered; however, awnings and umbrellas are permissible and encouraged.
  - iii. The width of a forecourt shall not be more than fifty (50) percent of the main building frontage.
  - iv. Forecourts shall be paved and enhanced with landscaping.
  - v. A fence or street wall at the façade line may be used to define the private space of the court.
  - vi. The court may be raised from the sidewalk, creating a small retaining wall at the primary façade line with entry steps to the court.
- e. **Stoops and porches.** Stoops are elevated entry porches/stairs placed close to the frontage line where the ground floor is elevated from the sidewalk, securing privacy for the windows and front rooms. Porches are commonly associated with residential buildings, wherein the façade is set back from the frontage line.
  - vii. Stoops shall correspond directly with the building entry.
  - viii. Porches may be one (1) or two (2) stories.
  - ix. Stoops and porches are permitted to extend into the street setback.
  - x. Porches shall be open and not air conditioned.



**Figure 41: Building Frontage Design**

Storefront, awnings/marquees	Gallery	Arcade
T-4, T-5, T-6	T-4, T-5, T-6	T-4, T-5, T-6
		
<p>① Width: 25% of façade width min.                  ② Depth: 5' max.                  ③ Awning/marquee Clear Height: 8' min. (9' over the ROW)</p>	<p>① Width: 75% of façade width min. but no less than 10' wide                  ② Depth: 10' min.                  ③ Clear Height: 12' min. (1st floor)</p>	<p>① Width: 75% of façade width min. but no less than 10' wide                  ② Depth: 10' min.                  ③ Clear Height: 12' min. (1st floor)</p>
		

<b>Forecourt</b> <b>T-4, T-5, T-6</b>	<b>Stoop</b> <b>T-3, T-4, T-5, T-6</b>	<b>Porch</b> <b>T-3, T-3.5</b>
		
<p>① Width: 10' min. to 50% of façade width max.</p> <p>② Depth: 10' min./20' max.</p> <p>③ Elevation: 18" max. above grade.</p>	<p>① Width: 5' min. to 16' max.</p> <p>② Depth: 5' min. to 8' max.</p> <p>③ Clear Height: 8' min.</p> <p>④ Elevation: 18" min. above grade.</p>	<p>① Width: 12' min.</p> <p>② Depth: 6' min.</p> <p>③ Clear Height: 8' min.</p> <p>④ Elevation: 18" min. above grade.</p>
		

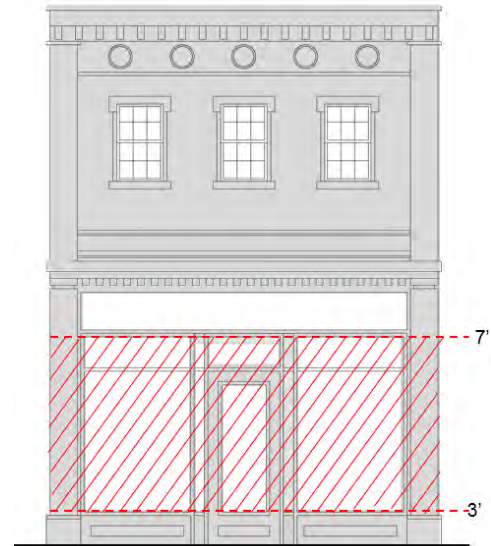


## 15. WINDOWS AND DOORS/FENESTRATION. (+)

The arrangement of windows and doors on new buildings varies depending on the building typology and the transect zone where it is located. Buildings in the T-4, T-5 and T-6 zones must be consistent with the main street typical pattern of windows and doors, which includes storefront-type openings on the first floor and more conservative openings on the upper stories. Buildings in the T-3.5 and T-3 zones are more residential in character and do not need to provide as much fenestration. Development on all t-zones shall meet the following standards:

- a. All buildings are subject to minimum fenestration and glass transparency requirements regardless of the street they front. The fenestration percentages shall be calculated as the total area of glass (windows and glass doors), or openings in the case of parking garages, divided by the façade area as follows.
  - i. Non-Residential First Floor: In the T-4, T-5 and T-6 zones, the area of glass from three (3) feet to seven (7) feet above grade (as illustrated in Figure 42) shall have no less than 30% of that area dedicated to fenestration. (+)
  - ii. Upper Floors: The combined area of glass on all floors above the first divided by the total area of the building façade for those floors shall be no less than 15%.(+)
  - iii. Residential: Buildings in the T-3.5 and T-3 zones are more residential in character but must provide be no less than 15%. (+)

**Figure 42. Measuring Fenestration**



**Figure 43. Inappropriate Fenestration**





**Figure 44. Appropriate Fenestration**

- b. If a façade is proposed to be predominantly glass, an architectural feature, such as a marquee, overhang, or a horizontal change in plane shall be provided between the first and second floors to ensure pedestrian scale at the sidewalk level.
- c. Shutters shall be sized to match the corresponding window openings. Their shapes, materials, proportions, design, and hardware also need to be in character with the style of the building.
- d. Windows shall not be blacked-out or covered with signage, furniture, or similar elements for at least six (6) feet behind the window.
- e. Windows and doors should be glazed in clear glass with no more than ten (10) percent daylight reduction (transmittance) for retail establishments, and fifty (50) percent for office and residential uses. Glass block is not considered transparent and shall not count toward the minimum fenestration requirement.
- f. Reflectance shall not exceed fifteen (15) percent.
- g. Stained or art glass shall only be permitted if consistent with the style of the building (churches, craftsman buildings, etc.).

## 16. BUILDING MATERIALS.

Facade materials visible from the street should not be selected on the basis of cost alone, but rather on compatibility with the building style and neighborhood character.

- a. Encouraged Façade Materials
  - Concrete masonry units with stucco
  - Reinforced concrete with stucco
  - Brick
  - "Hardie-Plank" siding
  - Wood lap board siding
  - Stone
- b. Prohibited Façade Materials:
  - Cedar shakes, wood shingles or shakes

- Metal/steel walls
- Corrugated or reflective metal panels (not intended to prohibit metal roofs or architectural accents)
- Unfinished block
- Textured plywood
- Mirrored glass
- Pre-engineered metal buildings
- Plastic siding
- Tile (except as an architectural accent)
- Chain link fencing
- Polyurethane and polystyrene foam products (except as an architectural accent)

## 17. BUILDING COLORS.

- No less than two (2) and no more than three (3) different colors or color shades (one primary/body color, and no more than 2 accent/trim colors) shall be used on a single building.
- Building, trim and detail colors must be complementary and shall not be used for advertisement.
- Building colors should be selected based on the architectural style of the building.
- Black, fluorescent, or neon as the predominant exterior color is prohibited
- A solid line band of color or group of strips, without a change in plane or material, shall not be used for architectural detail.
- All building facades, including those not facing a street, shall use the same color scheme.

**Figure 45. Appropriate Colors**



**Figure 46. Inappropriate Color Schemes****18. ROOF DESIGN.**

- a. Roofs shall be in scale with the building mass. Therefore, the height of the roof shall not exceed the height of the supporting walls.
- b. Building elevations shall have a defined top edge consisting of, but not limited to, cornice treatments, roof overhangs, brackets, and/or stepped parapets. Colored stripes/bands on flat roofs are not acceptable as a recognizable top. The cornice shall be at least eighteen (18) inches in height. Parapet shall be a minimum of two (2) feet in height.
- c. Roof materials shall be light-colored or a planted surface (green roof).
- d. Parapets shall be three dimensional and shall incorporate a minimum return of six (6) feet (see Figure 47). (+)

**Figure 47. Prohibited Parapets**

- e. Cornices shall be expressed with greater than flashing or minimal materials. (+)
- f. Prohibited roof types and materials:
  - i. Mansard roofs and canopies, unless used in conjunction with Parisian style mansards (see Figure 48).



- ii. Roofs with a 2/12 pitch or less without full parapet coverage
- iii. Back-lit awnings used as a mansard roof

**Figure 48. Mansard Roofs**

*Allowed:*

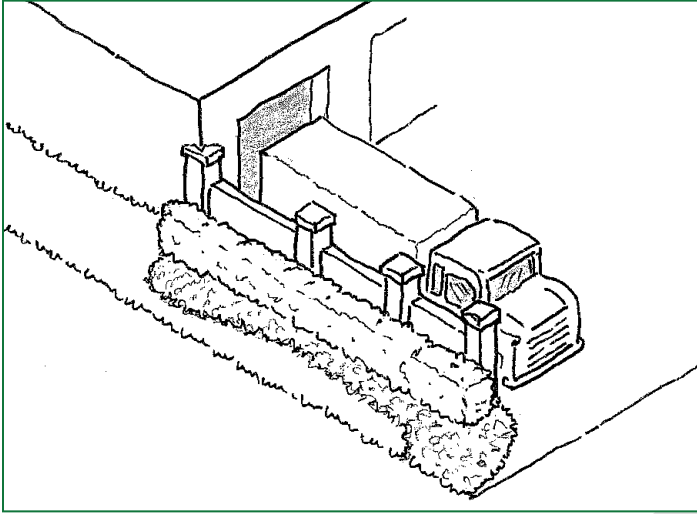


*Prohibited:*



## 19. UTILITIES/SERVICE AREAS.

- a. Loading areas, outdoor storage, waste disposal, mechanical equipment, satellite dishes, truck parking, and other service support equipment shall be located behind the building line and shall be fully screened from the view of the street and adjacent properties both at ground and rooftop levels.
- b. To the extent possible, areas for outdoor storage, trash collection, and loading shall be incorporated into the primary building design (see Figure 49) and shall not be located within 50 feet of single family residential lots. Dumpster pad enclosures shall be provided and must be constructed of masonry with a wooden or metal gate. Chain link is not permitted.

**Figure 49. Loading Area Screening**

- c. Mechanical equipment shall be screened according to city code; they should be integrated into the overall mass of a building by screening it behind parapets or by recessing equipment into hips, gables, parapets, or similar features (see Figure 50).

**Figure 50. Rooftop Units Screening***Allowed**Not Allowed*

- d. Shopping cart storage shall be located inside the building or shall be screened by a four (4) foot wall consistent with the building architecture and materials.
- e. Shopping cart corrals in the parking lot shall have no signage and shall not take parking spaces that are required to meet the minimum parking requirements.
- f. Electrical transformers and other utility equipment shall be screened from public view on all sides and fully screen horizontally.



## J. ACCESS, CIRCULATION AND PARKING REQUIREMENTS (\*)

### 1. SITE ACCESS.

- a. The number of curb cuts and driveways along Orange Avenue and Michigan Street shall be minimized. Properties along these roads shall explore the feasibility of connecting to adjacent sites rather than providing their own driveways. The standards of LDC Chapter 61, Part 1A regarding driveway separation shall be met. Cross-access easements shall be required unless exempted by the Planning Official.
- b. Pedestrian access from the public sidewalk shall be required for all development consistent with the Traditional City standards.

### 2. ON-SITE CIRCULATION.

- a. New developments must seek to create a balanced transportation system that accommodates pedestrians, bicyclists, and transit riders, as well as motor vehicles, and provides a system of connections to maximize choices for all modes of travel. Figure 51 provides an illustrative example of pedestrian and parking linkages.

**Figure 51. Pedestrian Linkages**



- b. The use of joint access driveways is required between commercial sites to reduce the number of access points and driveways that cross sidewalks.
- c. Safe pedestrian connections shall be provided not only along the perimeter of the blocks but also throughout the interior of development sites.
- d. Direct pedestrian access shall be provided from the principal entrance of the building to the sidewalk on the closest public right-of-way.
- e. Additional/secondary pedestrian access to businesses may be provided from parking facilities directly to ground floor uses, either through rear building entrances, sidewalks along the perimeter of buildings, or by pedestrian alleyways which connect the rear parking lots to the



sidewalks along the front street. Pedestrian passageways may be exterior and located between buildings, or may be incorporated into the interior design of a structure. The pedestrian alleyways shall be a minimum of six (6) feet wide.

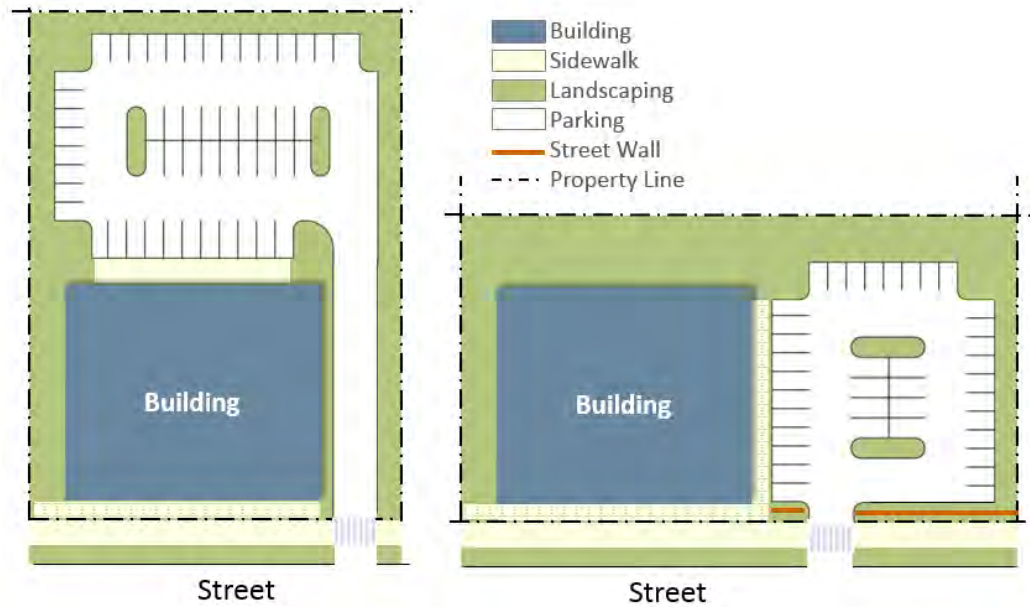
- f. Pedestrian walkways within the development shall be differentiated from driving surfaces through a change in materials and/or grade elevation.
- g. Crosswalks are required wherever a pedestrian sidewalk or alleyway intersects a vehicular area. All crosswalks shall be a minimum of five (5) feet wide and shall be paved with concrete modular paving or integrally poured concrete.
- h. All new development is encouraged to provide connections to existing bike and pedestrian pathways.
- i. Installed bicycle racks shall be designed to accommodate the minimum number of bicycles as set forth in this section.
- j. Bicycle racks shall be permanently fixed to the ground through mechanical fasteners or through the use of concrete footings.
- k. The design of bicycle racks placed within the development site shall coordinate with the overall design scheme of the subject site in terms of color and material. Bike racks placed within the right-of-way shall be consistent with the SODO Complete Streets Master Plan.
- l. Developments of 50,000 gross square feet or more may be required to provide access for on-site public transit.

### 3. LOCATION OF PARKING FACILITIES.

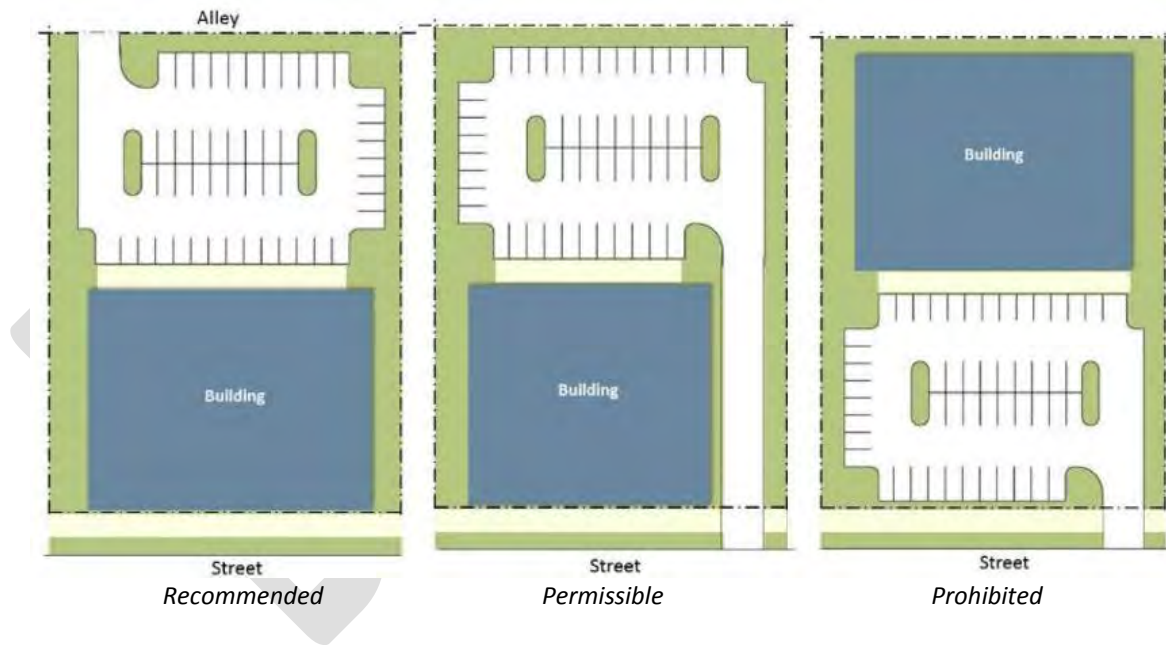
- a. Surface parking lots shall be located behind buildings so that the parking areas are screened from public sidewalks and streets consistent with the Traditional City standards.
- b. Where a site is too shallow to provide parking behind the building, the placement of parking facilities and vehicular driveways is permitted on the side of the proposed building (see Figure 52) only if the building setback requirements are met and a modification of standards for the minimum building frontage (if necessary to accommodate the parking area) is approved (see Section F, Modifications). The vehicular areas shall be screened from the road by a street wall (see Section K.3) with landscaping in front. Design conflicts between vehicular and pedestrian movement generally shall be decided in favor of the design which promotes pedestrian circulation.
- c. Surface and structure parking areas shall be accessed from a secondary street, from an adjacent property (joint access easement and shared use agreement necessary), or from rear alleys if any of these are available or proposed as part of the development (see Figures 53 and 54). Access through single family residential neighborhoods, however, shall not be allowed.
- d. Any surface parking areas located along a public street shall be screened from street view by a street wall. See wall standards in Section K.3.
- e. On-street parking directly in front of the site may be counted toward the minimum required parking, but only if the parking spaces were created as part of the development or redevelopment of the site. (+)

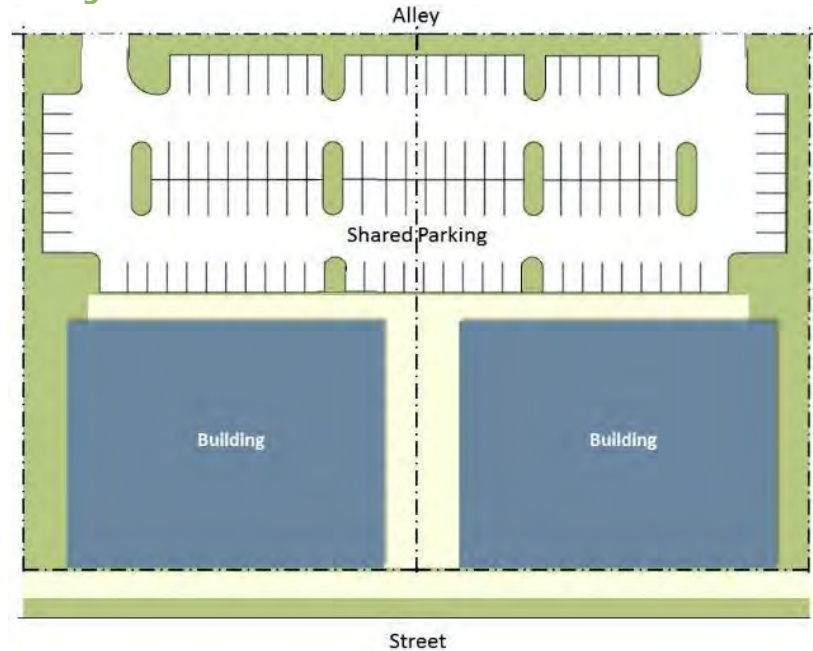
- f. Surface parking facilities to support uses in other T zones with frontage on Orange Avenue or Michigan Street may be allowed in T-3.5 provided that the standards of Section G.2.a are met.

**Figure 52: Parking on the Rear or Side**



**Figure 53: Site Access**



**Figure 54: Shared Parking.**

#### 4. PARKING GARAGES.

- a. Parking garages, as a principal or accessory use, shall require Conditional Use Permit.
- b. Parking structures facing Primary Streets (as defined in Section B and depicted in Figure 74) shall be placed behind a liner building that houses active uses.
- c. If the parking garage has more than one story and the building is located along a primary street, the liner building shall be at least two (2) stories in height and twenty (20) feet in depth.
- d. The liner building shall house active uses (e.g. commercial, office, residential) along the first floor facing the public right-of-way.
- e. The liner building shall extend for a minimum of seventy-five (75) percent of the length of the parking structure facing the primary public right-of-way.
- f. The liner building may be attached to or detached from the principal building they are concealing (see Figure 55) and may be in front of the parking structure or imbedded into the façade (see Figure 57).
- g. Liner buildings are not required along other streets. However, any portion of a parking garage that is not concealed behind a building shall be screened to conceal all internal elements such as plumbing pipes, fans, ducts and lighting and incorporate architectural treatments of the principal building. A buffer yard A shall be required on that side of the building (see LDC Section 60.218).
- h. Ramping shall be internalized. Exposed spandrels shall be prohibited (see Figure 56).



**Figure 55. Detached Liner Building****Figure 56. Exposed Spandrels****Figure 57: Parking Garages and Liner Buildings**

- a. Parking garages not concealed behind liner buildings or active uses (e.g. commercial, office, residential) shall provide a landscaping strip along the facade as follows:
  - i. The landscaping strip shall have a minimum depth of 8 feet; and
  - ii. A minimum of five (5) understory trees per 100 linear feet and a row of evergreen shrubs shall be planted along the strip. Vines growing on a metal mesh mounted on the wall of

the parking garage are allowed, provided they achieve eighty (80) percent coverage (Figure 58).

- b. The exterior facades of all parking garages shall be designed as to achieve architectural unity with the principal structure(s) which they are intended to serve.
- c. Parking structures shall meet setback, height, façade articulation and fenestration standards contained in this code.

**Figure 58: Garage Landscaping**



## 5. PARKING FOR SINGLE USES.

Off-street parking for developments that propose only one type of land use shall provide the number of spaces required in Tables 6 and 7 (per LDC Tables 26 and 27).

**Table 6: Parking Ratios for Residential Uses**

Use	Per Dwelling Unit	Per Rated Patron Capacity	Special Requirements
Accessory Apartments	1		
Assisted living facilities		0.2	Plus 1 space
Attached Dwellings and Multiplexes (up to 7 units)	1		Plus 1 space for each dwelling unit over 2,000 sq. ft. of gross floor area
Duplexes and Tandems	1		Plus 1 space for each unit over 1,500 sq. ft. GFA
Group housing		0.5	Plus 2 spaces
Mobile home dwellings	1		
Multi-family dwellings:			For government assisted elderly housing, these standards shall be reduced by 40%. For housing in a mixed-use development, the number of spaces per unit may be reduced by 0.25 within a one-half mile radius of a commuter rail station, or by up to 25% with a Conditional Use Permit.
- Efficiency apt.	1		
- Studio & 1-bedroom	1.5		
- 2-bedrooms	1.75		
- 3 or more bedrooms	2		
Nursing homes		0.5	
One family dwellings	1		
Residential care facilities:			
- 6 or fewer residents	1		
- 7 or more residents		0.2	Plus 1 space
Senior Living Facilities:			
- Assisted Care Units		0.2	
- Nursing Care Units	0.5		
- Independent Care Units	1		

Source: LDC Table 26.

**Table 7: Parking Ratios for Non-Residential Uses**

Land Use	Minimum Requirement	Maximum Permitted
Amusement centers	1:7 RPC	1:3 RPC
Auditoriums	0.10:seat	0.30:seat
Auto service stations <sup>1</sup>	2:service bay	No maximum.
Banks & savings institutions	2.5:1000 sf GFA	7:1000 sf GFA
Beauty & barber shops	5:1000 sf GFA	15:1000 sf GFA
Bed & breakfast facilities <sup>1</sup>	1:lodging unit	No maximum.
Bowling lanes	4:lane	5.5:lane
Churches & religious institutions (*includes all principal buildings, e.g. Sunday school, rectory, etc.)	2:1000 sf GFA or 0.20:seat (the greater standard shall apply)	No maximum.
Civic clubs & lodges <sup>1</sup> (*includes outside improved areas used for assembly, recreation, etc.)	2.5:1000 sf GFA	No maximum.
Community centers	2.5:1000 sf GFA	5:1000 sf GFA
Contractors & trade shops	1.5:1000 sf GFA	3.5:1000 sf GFA
Convention halls	3.5:1000 sf GFA	7.5:1000 sf GFA
Dance studios	2.5:1000 sf GFA	4:1000 sf GFA
Day care centers - child, adult	1:12 RPC	1:7 RPC
Discotheques & dance halls <sup>1</sup>	5:1000 sf GFA	20:1000 sf GFA
Eating & drinking establishments	5:1000 sf GFA	20:1000 sf GFA
Funeral homes (*plus 25:1000 sf area used for temporary seats or standing, plus spaces for vehicles operated by the establishment)	0.25:seat	0.5:seat
Furniture stores	1.2:1000 sf GFA	2:1000 sf GFA
Game rooms	2.5:1000 sf GFA	4:1000 sf GFA
Golf courses, miniature <sup>1</sup>	1.1:hole	1.8:hole
Health spas	2.8:1000 sf GFA	5.6:1000 sf GFA
Hospitals (* plus spaces for emergency vehicles, etc.)	2.3:1000 sf GFA or 0.8:bed	No maximum.
Hotels & motels - total all of the following:		
- Guest Rooms	0.5:lodging unit	1:lodging unit
- Restaurant, cocktail lounge	5:1000 sf GFA	10:1000 sf GFA
- Banquet/meeting rooms	0.25:seat	0.5:seat
- Other uses: ½ the number of spaces ordinarily required by the land use category		
Laboratories, medical & dental	2.5:1000 sf GFA	4:1000 sf GFA
Libraries	2.5:1000 sf GFA	4:1000 sf GFA
Manufacturing & processing	1.5:1000 sf GFA	3.5:1000 sf GFA
Museums & art galleries	2.5:1000 sf GFA	4:1000 sf GFA
Offices - general, government, postal	2.5:1000 sf GFA	4:1000 sf GFA
Offices/Clinics - medical, dental	2.8:1000 sf GFA	5.3:1000 sf GFA
Offices - telemarketing, call center	2.5:1000 sf GFA	6:1000 sf GFA
Open air markets	2.5:1000 sf BSA	4:1000 sf BSA
Personal storage facilities (plus a minimum of 3 spaces at the manager's office)	Refer to Sec. 58.773	No maximum.



Land Use	Minimum Requirement	Maximum Permitted
Retailing - light, intensive & shopping centers:		
- < 4,000 sf GFA	2.5:1000 sf GFA	5:1000 sf GFA
- 4,000 - 400,000 sf GFA	2.5:1000 sf GFA	4:1000 sf GFA
- 400,001 - 600,000 sf GFA	3:1000 sf GFA	4.5:1000 sf GFA
- > 600,000 sf GFA	3.5:1000 sf GFA	5:1000 sf GFA
Schools - elementary & middle <sup>1</sup>	1.75:classroom and 1:5 auditorium or gymnasium seats	No maximum.
Schools - high school, vocational or business school, college or university <sup>1</sup>	5:classroom and 1:3.5 auditorium or gymnasium seats	No maximum.
Services, business	2:1000 sf GFA	3.5:1000 sf GFA
Services, entertainment	1.5:1000 sf GFA	3.5:1000 sf GFA
Services, intensive	1.5:1000 sf GFA	3.5:1000 sf GFA
Services, personal	2.5:1000 sf GFA	4:1000 sf GFA
Services, major vehicle repair <sup>1</sup>	2.5:service bay	No maximum.
Stadiums	1:7 RPC	1:3 RPC
Temporary labor services	4:1000 sf GFA	No maximum.
Temporary professional services	2.8:1000 sf GFA	4:1000 sf GFA
Theaters	0.10:seat	0.30:seat
Vehicle sales & rental <sup>1</sup> (* plus 1:1000 additional sf GFA)	4:1 <sup>st</sup> 3000 sf GFA of office, showroom, display of goods, servicing	No maximum.
Warehouse/showroom	2:1000 sf GFA	4:1000 sf GFA
Warehousing & storage	0.25:1000 sf GFA	3.5:1000 sf GFA
Whole blood & plasma facilities	1.3:operating station	No maximum.
Wholesale distribution	1.5:1000 sf GFA	3.5:1000 sf GFA

Source: LDC Table 27.

## 6. PARKING FOR MIXED-USE DEVELOPMENTS. (+)

When any land or building accommodates two (2) or more categories, the maximum amount of parking allowed shall be divided by the appropriate factor from the Sharing Factor matrix (Table 8). When more than two uses share parking, the lowest number shall be used. The required number of handicap spaces, however, shall not be reduced.

**Table 8: Sharing Factor Matrix**

Function	with		Function
RESIDENTIAL			RESIDENTIAL
LODGING			LODGING
OFFICE			OFFICE
RETAIL			RETAIL
		1	
	1.1	1.1	
	1.4	1	1.4
	1.2	1.7	1.2
	1.3	1	1.3
	1.2	1.2	
	1		

## 7. PARKING LAYOUT.

Parking space and aisle dimensions in all facilities shall conform to the dimensional requirements of Section 61.309, summarized below, and Figures 17 and 18 of the LDC (shown below as Figures 59 and 60).

- a. **Parking Stall Width:** Parking stalls shall have a minimum width of 9 feet, except that a width of 8 feet 6 inches may be permitted for low turnover spaces. The module (M) and the associated aisle width (A) may be reduced by up to 2 inches for each 1 inch of additional stall width (SW) to a maximum width of 9 feet 6 inches. Where walls, columns, and other vertical obstructions are located immediately adjacent to the sides of parking stalls, the stall width shall be increased by a minimum of 1 foot.
- b. **Low Turnover Parking Spaces:** Up to 20% of the required parking spaces at shopping centers may be designated as low turnover spaces having a minimum stall width of 8 feet 6 inches. The 8-foot 6-inch stall width shall only be permitted in locations intended and suitable for employee parking or in areas likely to be used only on peak shopping days.
- c. **Aisles.** Aisles for 90-degree parking spaces shall be designed to accommodate two-way traffic flow. Aisles for all other angles shall be designed to accommodate one-way traffic flow. For two-way traffic flow, the minimum aisle width (A) shall be 24 feet.
- d. **Turning Bays.** In all parking facilities, turning bays shall comply with the minimum dimension of 16 feet 6 inches, as shown in Figure 60 below. For two-way traffic flow, the minimum turning bay width shall be 24 feet.

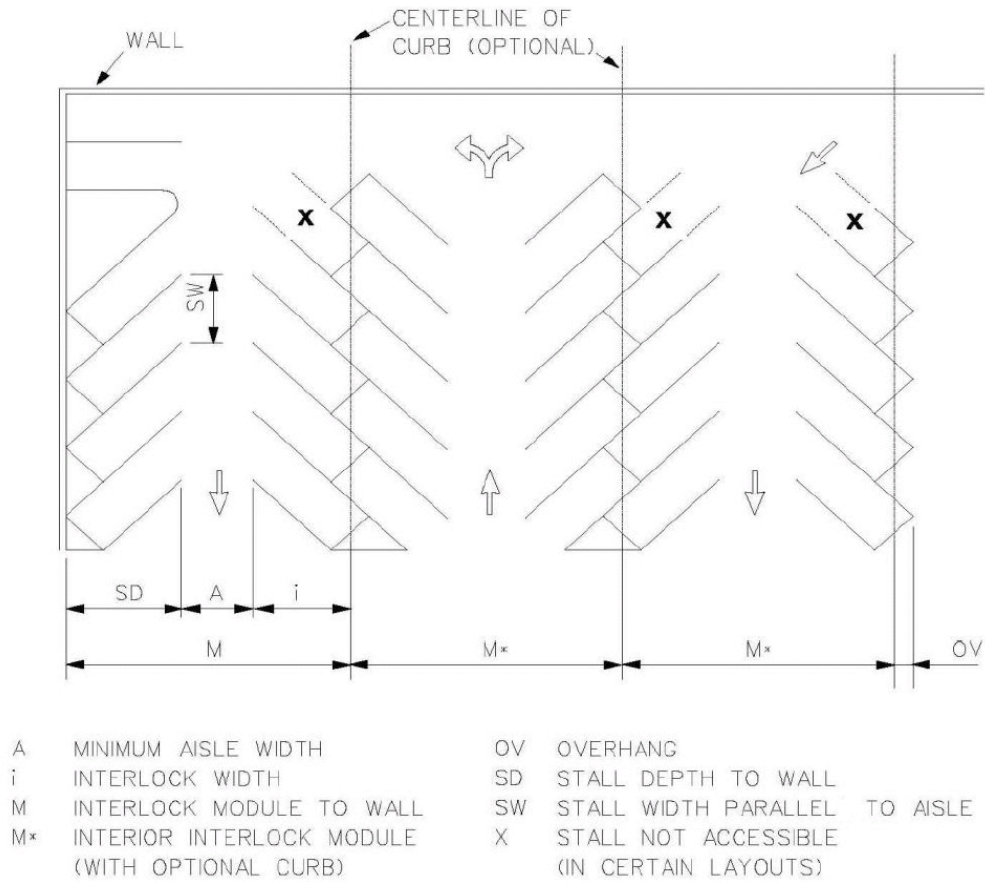
**Table 9. Required Parking Dimensions.**

Angle of Parking	Stall Width (SW)		Stall Depth (SD)	Aisle Width (A)		Interlock (i)		Overhang (OV)	Module (M)		Module (M*)	
	9' 0"	8' 6"		9' 0"	8' 6"	9' 0"	8' 6"		9' 0"	8' 6"	9' 0"	8' 6"
45°	12' 7"	12' 0"	19' 6"	13' 0"	14' 0"	16' 0"	16' 6"	2' 0"	48' 6"	45' 0"	45' 0"	47' 0"
60°	10' 4"	9' 10"	20' 6"	15' 0"	16' 0"	18' 0"	18' 6"	2' 0"	53' 6"	55' 0"	51' 0"	53' 0"
75°	9' 3"	8' 10"	20' 0"	19' 6"	20' 6"	18' 6"	19' 0"	2' 0"	58' 0"	59' 6"	56' 6"	58' 6"
90°	9' 0"	8' 6"	18' 6"	23' 0"	24' 0"	18' 6"	18' 6"	2' 0"	60' 0"	61' 0"	60' 0"	61' 0"

Notes:

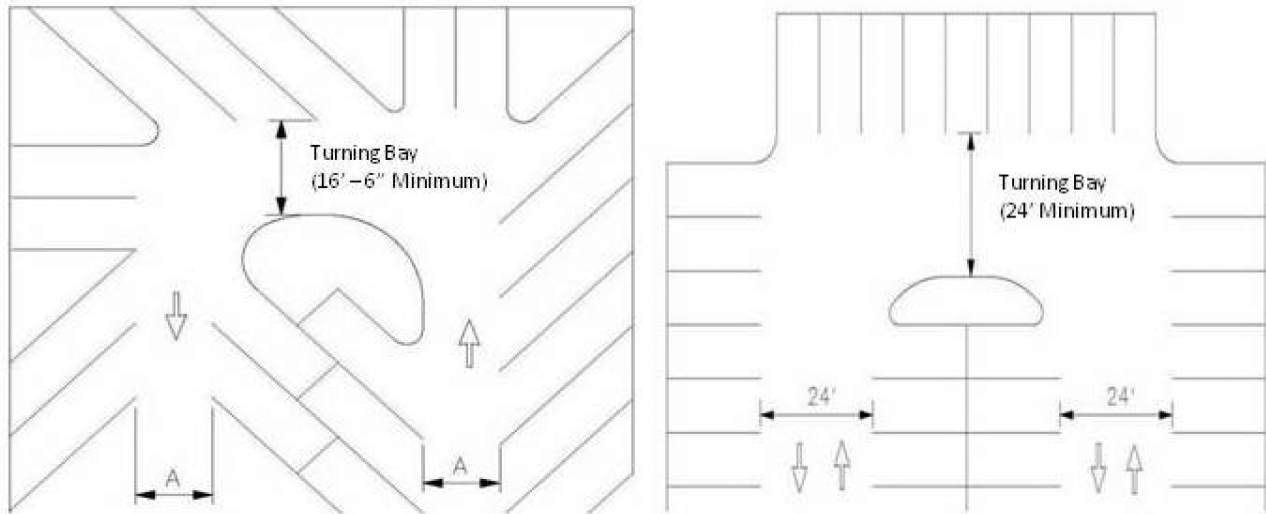
1. 90-degree parking is preferred.
2. Parking angles from 76 to 89 degrees are not permitted.
3. Parallel parking stalls shall have a minimum length of 23 ft. 0 inches and a minimum width of 9 ft. 0 inches.
4. The Transportation Official shall be authorized to allow a maximum 1-foot reduction of the module (M) and the associated aisle width (A), based on City of Orlando Engineering Standards Manual requirements.

**Figure 59. Parking Layout Terms**



Source: LDC Figure 18.

**Figure 60. Minimum Turning Bay Dimensions**



Source: LDC Figure 19.



## 8. EXISTING NON-CONFORMING PARKING FACILITIES.

There are several sites in the SODO district with parking spaces that do not meet current code standards in terms of access, location or size.

- a. Parking in Front of the Building. In conjunction with the redevelopment of a site, parking facilities that are already located in front of a building that is not being moved or demolished (see Figure 61) may remain as long as the parking spaces are determined to be safe by the City.
- b. Curb Cuts and Driveways. Non-conforming curb cuts and driveways will only be required to meet the standards of this section if there is substantial redevelopment or a change of use on the site, as defined in Section C.

**Figure 61. Existing Non-Conforming Parking Spaces**



## K. LANDSCAPE, BUFFERING AND SCREENING

Landscape, buffering and screening shall be provided as required by Chapters 60 and 63 of the LDC, and the following:

**Figure 62. Planters Adjacent to Buildings**



### 1. LANDSCAPE MATERIALS.

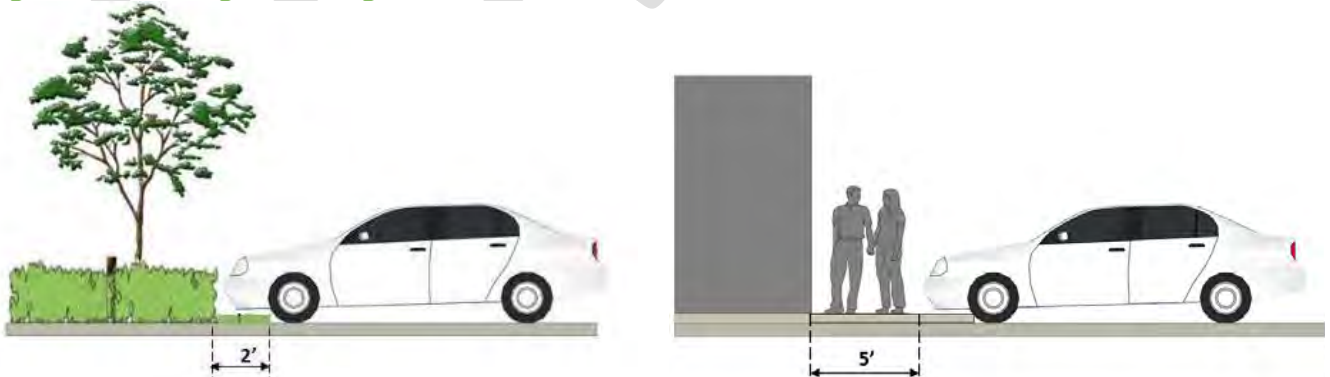
At the time of planting, trees within rights-of-way and public parking areas shall meet the following standards:

- a. Canopy trees that are planted closer than five (5) feet from the back of a curb, sidewalk, or building shall be planted with a root barrier control method approved by the City.
- b. Tree and site lighting locations shall not be in conflict as required by the lighting code.

### 2. PARKING LOT LANDSCAPING AND LIGHTING.

- a. Parking lot landscaping shall meet the standards of Section 61.312, and the alternative design shown in Figure 23 of the LDC.
- b. Parking lot layout, landscaping, buffering, and screening should minimize direct views of parked vehicles from streets and sidewalks; prevent spill-over light, glare, noise, and exhaust fumes from infringing on adjacent properties; and provide the required tree canopy shade.
- c. Any landscaping adjacent to the front of a parking space shall include a two (2) foot wide strip of gravel, grass or mulch so that the front of the parked vehicle can overhang without interfering with taller plantings (see Figure 63).
- a. Parking spaces adjacent to a sidewalk are permitted as long as the car overhang does not reduce the width of the sidewalk below five (5) feet (see Figure 63).

**Figure 63: Parking Overhang**



- b. Parking lot light fixtures shall be designed so that light is directed onto the parking area and away from neighboring residential lots (e.g. house side shields).

- c. The design, color, shape, style, and materials of the fixtures shall match or complement the style and materials of the buildings served.
- d. Lighting for structure parking shall be shielded to the outside of the structure; top level lighting shall be placed at the interior of the floor, not to the edge of the structure.

### 3. BUFFER YARDS AND SCREENING.

Buffer yards shall be provided as required by the Land Development Code (see LDC Chapter 60, Part 2E), except that no buffer shall be required between the building and the street, or between similar non-residential uses unless specifically required as part of a Master Plan or Conditional Use review.

Urban street walls (see Figure 64) are the only type of fence/wall allowed facing streets and alleys. A street wall shall be required along the perimeter of a surface parking lot that abuts a street or alley. Whenever a street wall is provided, it shall meet the following standards:

**Figure 64. Street Wall Examples**



- a. The wall shall be a minimum of three (3) feet and a maximum of four (4) feet in height. The portion of the wall above three (3) feet shall be no more than fifty (50) percent solid. (+)
- b. Street walls shall be constructed of brick, masonry, stone, powder-coated aluminum or other decorative materials that complement the finish on the primary building. Wrought iron may be used for the portion of the wall between three (3) and four (4) feet in height. Chain link, wood and PVC street walls shall be prohibited. (+)
- c. When a parking lot abuts a public right-of-way, ground cover, hedges, or shrubs shall be installed on both sides of the wall. The landscaping strips shall be a minimum of three (3) feet wide. The area in front of a street wall may be landscaped or used to expand the public sidewalk (see Figure 65).
- d. Breaks along the street wall are required to provide pedestrian access to the site.
- e. Street walls may be interrupted for the purpose of tree protection.



Figure 65. Street Wall Location and Landscaping



## L. STORMWATER & SITE GRADING (\*)

If a master stormwater system is available to the site, the developer shall connect to that system, rather than provide on-site facilities. Existing on-site ponds on redevelopment sites shall be removed and connection to the master system shall be required. Development on sites not eligible to connect to a master system shall meet the following requirements. Nothing in this section shall exempt the applicant from meeting all requirements of the state, water management district, or other governing body for water quality standards.

1. All retention areas shall be incorporated into the overall design of the project.
2. Where slopes require fencing, only ornamental metal fencing will be allowed.
3. A master drainage plan and site and building section drawings shall be included in all master plan, conditional use, or planned development applications. The master drainage plan shall include data on the 25-year, 24-hour pre- and post-conditions and the 100-year flood elevation. The site section drawings shall show the finished floor elevations of all proposed buildings. The purpose of the Master Drainage Plan and site and building section drawings is to clearly show the relationship of the stormwater systems and necessary site grading to the buildings, sidewalks, ramps, parking areas, outdoor dining areas, landscaping, and other site design elements.
4. The following techniques shall be utilized when designing stormwater systems for **T-4, T-5, and T-6** lots:
  - a. Where site grading is required, the resulting finished floor elevation shall be sensitively integrated into the design of the site using appropriate landscaping, building design, or active uses that can be appropriately elevated 1 to 3 steps above the sidewalk grade such as outdoor dining spaces. No steps or railings shall be permitted to encroach within a sidewalk, public right-of-way, or city services easement.
  - b. All stormwater systems shall be designed to function as site amenities, or exfiltration shall be required. Green roofs, rain gardens, rain cisterns, or other green or low impact design stormwater techniques may be considered site amenities for the purpose of this requirement.
  - c. Wet bottom ponds may only be allowed subject to Conditional Use approval.
  - d. Where maximum setbacks are prescribed, applicants are encouraged to locate stormwater in the middle of the site between the rear of the building and the parking area in order to decrease the elevation of buildings adjacent to public sidewalks.
  - e. To reduce the size of stormwater ponds that are not designed as amenities, the use of porous concrete, underground storage, and exfiltration is greatly encouraged. Each of these options may increase the amount of developable land or undisturbed open space.
5. The following techniques shall be utilized when designing stormwater systems for **T-3.5** lots:
  - a. Must be designed with the appearance of a pocket park, with creative use of grading, retaining walls, swales, and landscaping to create a park-like appearance.
  - b. Slopes should be less than 4:1.
  - c. No gravel bottoms are allowed, they must be landscaped.



- d. Litter management is required.
  - e. Solid walls may be used to support grading or to screen parking areas from neighborhoods. All other fencing must be CPTED-style open metal fencing. Chain link fencing is prohibited.
6. Stormwater facilities in T-3 zones are only allowed to support uses in other T zones with frontage on Orange Avenue or Michigan Street. The T-3 lot must be consolidated with a T-4 lot as a single development site, and a Bufferyard B, to include a solid masonry wall, must be provided between the stormwater area and any directly adjacent residential lot.

**Figure 66. Example of Urban Stormwater Facilities**





## M. SIGNS

Development within the SODO district shall comply with the sign standards of this section in addition to the dimensional standards contained in Chapter 64 of the Land Development Code. If any sign standards in this document are in disagreement with the citywide sign code, the standards detailed in this document shall take precedent.

### 1. BUILDING SIGNS.

Office uses in the T-3.5 zone are allowed one building sign not to exceed the following size. No pole or ground signs shall be allowed. (\*)

- One tenant: Four (4) square feet
- Two to three tenants: Six (6) square feet
- Four tenants or more: Eight (8) square feet

Individual businesses in all other zones shall be allowed up to two (2) building signs per business frontage as follows.

#### a. Types of Signs Allowed

- i. **Wall Sign:** A sign that is attached flat to a building wall.



- ii. **Projecting Sign (Bracket Sign):** A sign which is affixed and displayed perpendicular to any building wall or structure.



- iii. **Marquee Sign:** A sign which is suspended from, attached to, supported from or forms a part of a marquee.



- iv. **Awning Sign:** A sign that is painted, silk-screened, stitched on, imprinted on or otherwise applied directly onto the fabric of the awning. (+)



- v. **Hanging Sign:** a sign that is placed under a marquee, awning or arcade, perpendicular to the building façade, and not intended to be seen by motorists.





- vi. **Window Sign:** A permanent sign affixed to, or painted on either face of a window or glass door that leads to the exterior of the building. Signs suspended within three (3) feet behind the glass are also deemed window signs.



- vii. **Rooftop Sign:** A sign erected, constructed, or maintained above the roof of any building. (+)



- i. **3D Signs:** Three-dimensional (free-form, sculpture, or other non-planar shape) signs affixed to a building (see Subsection M.2 (Freestanding Signs) for 3D pole signs). (+)



## b. Building Sign Standards

- i. Wall signs shall be either a panel or individual letters applied to the wall, shall not extend above the top of the wall where it is located, and in the case of two story buildings, it shall



be placed between the first and second floor windows. Cabinet signs and signs painted directly onto the façade are not allowed. Push Through signs, however, are allowed.

- ii. Wall signs shall not extend closer than two (2) feet to the side edges of the façade.
- iii. Wall signs shall be permitted above the second story provided they are attached flat against the building wall and placed on the top floor of the building (see Figure 67). If they are located above thirty (30) from the ground, they shall conform to the high-rise sign regulations.

**Figure 67. Wall Sign Above Second Story**



- iv. Projecting signs may be read horizontally or vertically and may extend into the front or street side building setback, and the sidewalk provided they are setback 4 ft. from the curb and provide a 9 ft. clearance over the sidewalk.
- v. Marquee signs shall not exceed 75 percent of the width of the marquee. They are allowed to be placed fully or partially above or below the edge of the marquee (see Figure 68), provided the sign consists of individual letters (as opposed to a panel). Marquee signs are also allowed to hang from the bottom of the marquee facing the street.

**Figure 68. Marquee Signs extending above and below the marquee**



- vi. Awning signs may extend up to 75 percent of the width of the awning but shall not cover more than 30% of the surface of the awning facing the street.
- vii. Awning signs are only allowed on the vertical portion of the awning. They are not allowed on the sloping or curved section.
- viii. Window signs (silk screen, vinyl letters, gold leaf, hand painted or neon) shall not occupy more than 25% of the glass window or door and may be allowed for first and second story businesses.

- ix. Hanging signs shall not count toward the maximum sign area allowed, provided they are placed under the awning or marquee, perpendicular to the building, and near the front door of the business. Such signs shall have an 8-foot minimum vertical clearance as measured from grade to the bottom of the sign, and a maximum height of 2 feet.
- x. Signs shall not obscure architectural details such as windows, cornice, decorative brickwork and storefronts. No portion of a building sign shall extend below the lowest point of the roof or above the top edge of the parapet of the building to which it is attached.
- xi. Rooftop signs shall be limited to individual letter signs and are only allowed west of Orange Avenue and on buildings on the east side of Orange Avenue provided the signs face west. No cabinet signs shall be allowed as rooftop signs.
- xii. Rooftop signs shall not exceed fifteen (15) feet in height, or one third (1/3) of the building height, whichever is less.
- xiii. 3D signs are permitted. The area of a three-dimensional sign shall be calculated by adding the area of the four faces of the smallest cube or rectangular prism which could enclose the entire sign volume divided by two.

### c. Building Sign Illumination

Signs may have interior or exterior illumination sources (see Figure 69). Signs with interior illumination are limited to individual letters or push-through lettering. There shall be no illuminated signs facing a single-family home. Existing cabinet signs that change messages shall have opaque backgrounds.

**Figure 69. Permitted Sign Illumination**





## 2. FREE STANDING SIGNS. (+)

Freestanding signs, in the form of pole or monument signs are not permitted. However, the following signs may be allowed by the SODO TDR.

### a. Street Wall Signs:

Signs on street walls are allowed as follows.

- i. Street wall signs shall be placed flat on the street wall facing the street.
- ii. The sign shall not exceed a maximum of 36 square feet.
- iii. The sign shall not exceed a height of 6 feet measured from the ground (see Figure 70).

Figure 70. Street Wall Signs



### b. 3D Freestanding Sign Conversions:

Non-conforming freestanding (pole or monument) signs may be allowed to remain if they are converted to a 3D (non-planar shape) pole sign. The sign may include a cabinet or panel, but it must also include a three-dimensional figure to qualify. The total sign area shall not exceed the area allowed per Code. Figure 71 shows examples of appropriate 3D pole signs (left photo) and signs that would not qualify for this incentive (right photo). No sign movement shall be allowed.



Figure 71. 3D Freestanding Signs



3D Pole Sign



Not 3D



3D Pole Sign



Not 3D

## N. DEVELOPMENT BONUSES (+)

Developments in the SODO district may be eligible for building height and/or intensity/density bonuses as follows.

### 1. BONUSES AVAILABLE.

Development projects within the SODO district that meet all the standards contained in this document may be eligible to achieve the additional density and/or intensity noted below and the additional building height listed in Table 4 through the Master Plan process. Additional residential density may be approved by Master Plan in the industrial zoning districts.

**Table 10. Density/Intensity Bonuses**

Geographical Area (see Figure 72)	Max. Development Density (units per acre)	Max. Development Intensity (Floor Area Ratio)
12.8 (Kaley Gateway)	100	2.0
12.9 (Transit 1)	100	1.6
12.9.1 (Transit 2)	100	3.0
12.7 (General)	60	1.0

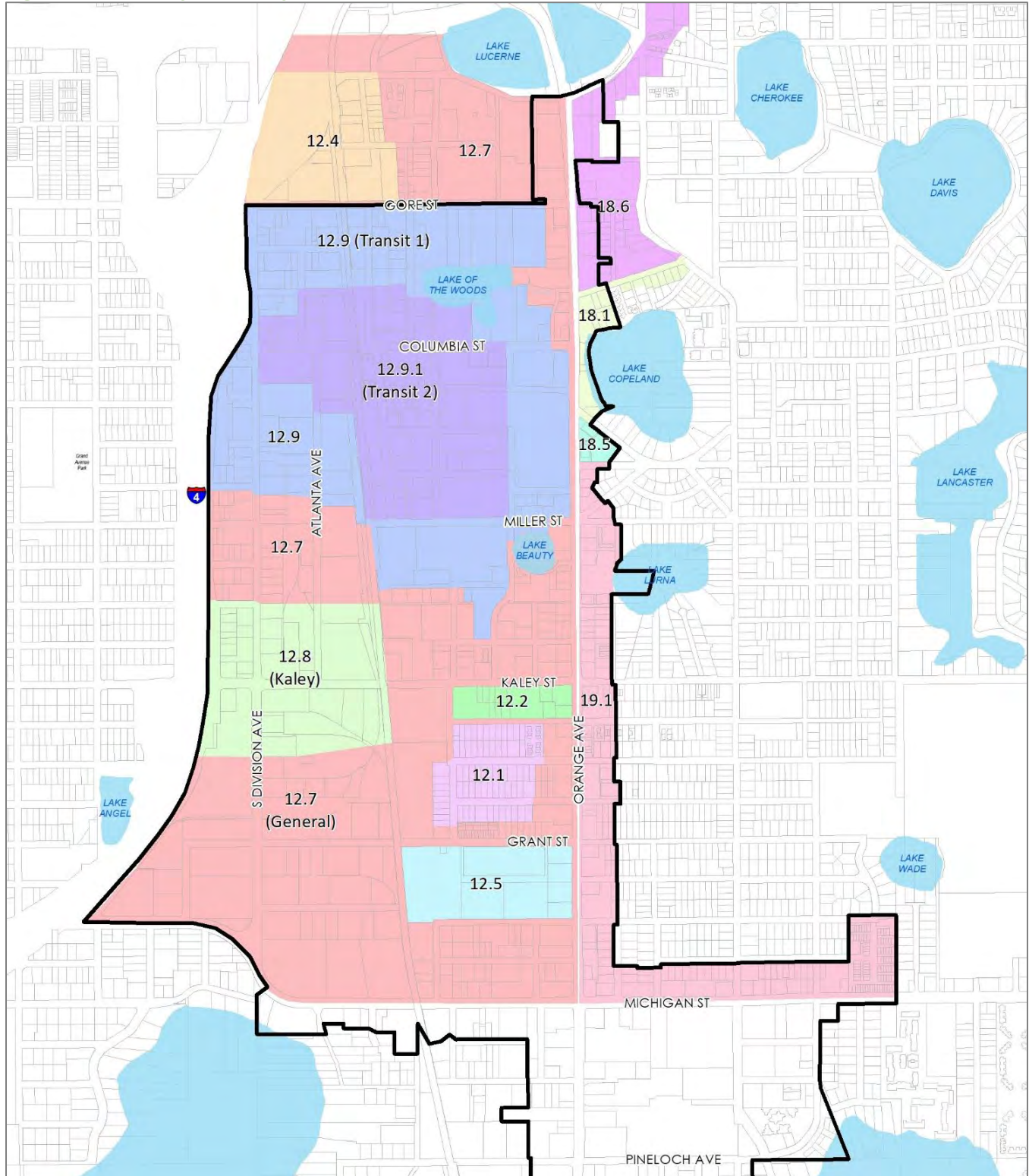
Density bonuses within the T-3.5 zone may only be approved when the additional units will be retained as affordable or attainable housing units within an otherwise market-rate development. No bonus shall be approved that increases the building mass beyond what would otherwise be allowed.

### 2. CRITERIA FOR DENSITY/INTENSITY BONUS.

Per LDC Section 58.1103, a bonus may be granted only when an applicant presents clear and convincing evidence that the proposed design, density, intensity, and mix of uses will result in a superior development that is compatible with the surrounding neighborhood and achieves the following criteria. In addition, the project must meet one or more of the design enhancements provided in subsection 3 (below).

- a. Public goods and services to serve the proposed development must be available, made available by the applicant in proportion to the demand generated by the development, or included in a financially feasible plan.
- b. The development must include space for at least two of the following uses. The secondary use must comprise at least 10% of the building area. The secondary use may not be reserved for use only by the principal user (for example, a residents-only gym or an employee cafeteria). The secondary use must be a permitted use or a lawfully allowed conditional use in the zoning district.
  - (1) Multifamily residential
  - (2) Office
  - (3) Light retailing
  - (4) Personal service
  - (5) Eating and drinking
  - (6) Hotel
  - (7) Indoor recreation
  - (8) Public benefit use
  - (9) Child day care center

**Figure 72. Density/Intensity Bonus Areas**



Source: City of Orlando Growth Management Plan



- (10) Multifamily residential that has been certified affordable in accordance with the City of Orlando Affordable Housing Certification Process.
- (11) Live/work units, defined as units that includes a complete dwelling unit with kitchen and bathroom, as well as space suitable for running a business, provided that the business is a permitted or lawfully approved conditional use in the zoning district. To qualify as a live/work unit for the purposes of this part, the live/work unit must be occupied entirely by a single housekeeping unit.
- (12) Publicly accessible open space that is open to the sky and at least 2,500 square feet in area. To ensure that the open space is functional, reasonably contiguous, and consistent with applicable design regulations, the site plan for the open space is subject to review and approval as part of the master plan application.
- (13) Ground floor space that is suitable for retail uses. For the purposes of this part, "suitable for retail uses" means, at a minimum, ceiling heights of at least 16', depth of at least 40', and street facing facades must be at least 30% transparent between 3' and 7' above grade.
- c. The density, intensity, height, and bulk of the building or buildings must be generally compatible with the surrounding neighborhood while also advancing the applicable goals, objectives, and policies of the Growth Management Plan. For the purposes of this part, "compatible with the surrounding neighborhood" does not necessarily mean "identical to" or even "similar to" the surrounding neighborhood. Developments using a density and/or intensity bonus will often be, by their nature, more dense, more intense, or both more dense and more intense than the surrounding neighborhood, but general compatibility with the surrounding neighborhood can still be achieved by ensuring that the subject development presents a logical transition between itself and the surrounding neighborhood.
- d. The development must be consistent with the Land Development Code.

**Figure 73. Examples of Bonus Elements**



*Mixed-Use/Superior Architectural Design*



*Ground Floor Retail*



*Publicly Accessible Open Space*

### 3. DESIGN ENHANCEMENTS.

Per LDC Section 58.1104, the following design enhancements represent options for creating a superior development. While not a strict point-based system, a development that meets a greater number of these enhancements is eligible for a greater bonus than a development that meets only one or two enhancements. If improvements to the streetscape or other public property is part of a selected option, then such improvements must be maintained by the property owner or owners of the subject development unless appropriate maintenance obligations are accepted by the City.

- a. Streetscape treatment that exceeds the minimum standards normally required of the development's location.
- b. Landscaping that exceeds by at least 10% the minimum number of points required by Part 2H, Chapter 60 of the Code.
- c. At least one additional mobility strategy than the minimum number of strategies required by section 59.209 of the Code.
- d. Site design and building materials that exceed the minimum environmental sustainability requirements in place at the time of development. Such enhancements should consider energy efficiency, stormwater design, solar or other renewable energy sources, recycling, and sustainable materials options, all of which should be judged against the best-available technology and any relevant and generally accepted environmental certification programs.
- e. For a high-rise building, a roofline that is sculpted to create an interesting form and enhance the collective skyline, or alternatively, a green-roof, active roof deck, or solar panels while maintaining an attractive and cohesive architectural form.
- f. Superior architectural design, including all of the following, where applicable:
  - (1) Buildings on corner lots and buildings that terminate views incorporate features that emphasize their prominent location.

- (2) In mixed-use buildings, the ground floor must be differentiated from upper floors through the use of architectural treatments.
- (3) Use of durable, high-quality materials that are appropriate for the climate, such as stone, steel, glass, precast concrete, or masonry. Such materials and associated architectural features must wrap around all sides of the building that are visible from the public realm. Traditional stucco may be appropriate in a historic context or for a Mediterranean-style building. Buildings incorporating EIFS (Exterior Insulation Finishing System) or other faux-stucco finish for more than 10% of a street-facing façade do not meet this enhancement option.
- g. Underground utilities, if currently aboveground.
- h. Parking garage is located such that it is interior to the site or lined with habitable space along all street-facing facades. The habitable space must meet applicable transparency requirements and must be included in locations not otherwise required by code, in particular on upper floors of the building.
- i. Another enhancement not otherwise required by code that is proposed by the applicant and approved as part of the master plan. Examples might include a publicly accessible park or plaza, public art, a premium transit stop, or public parking.

#### **4. SUBMITTAL REQUIREMENTS.**

Per LDC Section 58.1102, in addition to the site plan submittal requirements, applications for bonuses shall also include:

- a. Perspective drawings, axonometric drawings, block-face elevations, or computer simulations that illustrate the proposed development in context with adjacent buildings and the surrounding area.
- b. Street sections that illustrate typical street dimensions, streetscape treatments, the height and bulk of the proposed development, and the relationship of the proposed development to buildings and structures on the opposite side of the street.
- c. An infrastructure analysis to demonstrate that adopted level of service standards are maintained or achieved for parks, schools, transportation, potable water, wastewater, stormwater, and solid waste.

#### **5. REVIEW AND APPROVAL.**

Density, intensity and building height bonus requests shall be reviewed by the Municipal Planning Board and the City Council. The request for bonuses shall be noted on the master plan application and shall be reviewed concurrently with the master plan to ensure all other requirements of the Code, including the requirements of this Chapter, are met. Applicants requesting approval of bonuses are still eligible to apply for variances and modification of standards.

#### **6. LOCATION RESTRICTION.**

Developments utilizing the bonus system shall not have any buildings located within 100 feet from a single-family residentially-zoned property.



## **O. STREET STANDARDS**

### **1. RESPONSIBILITY FOR IMPROVEMENTS.**

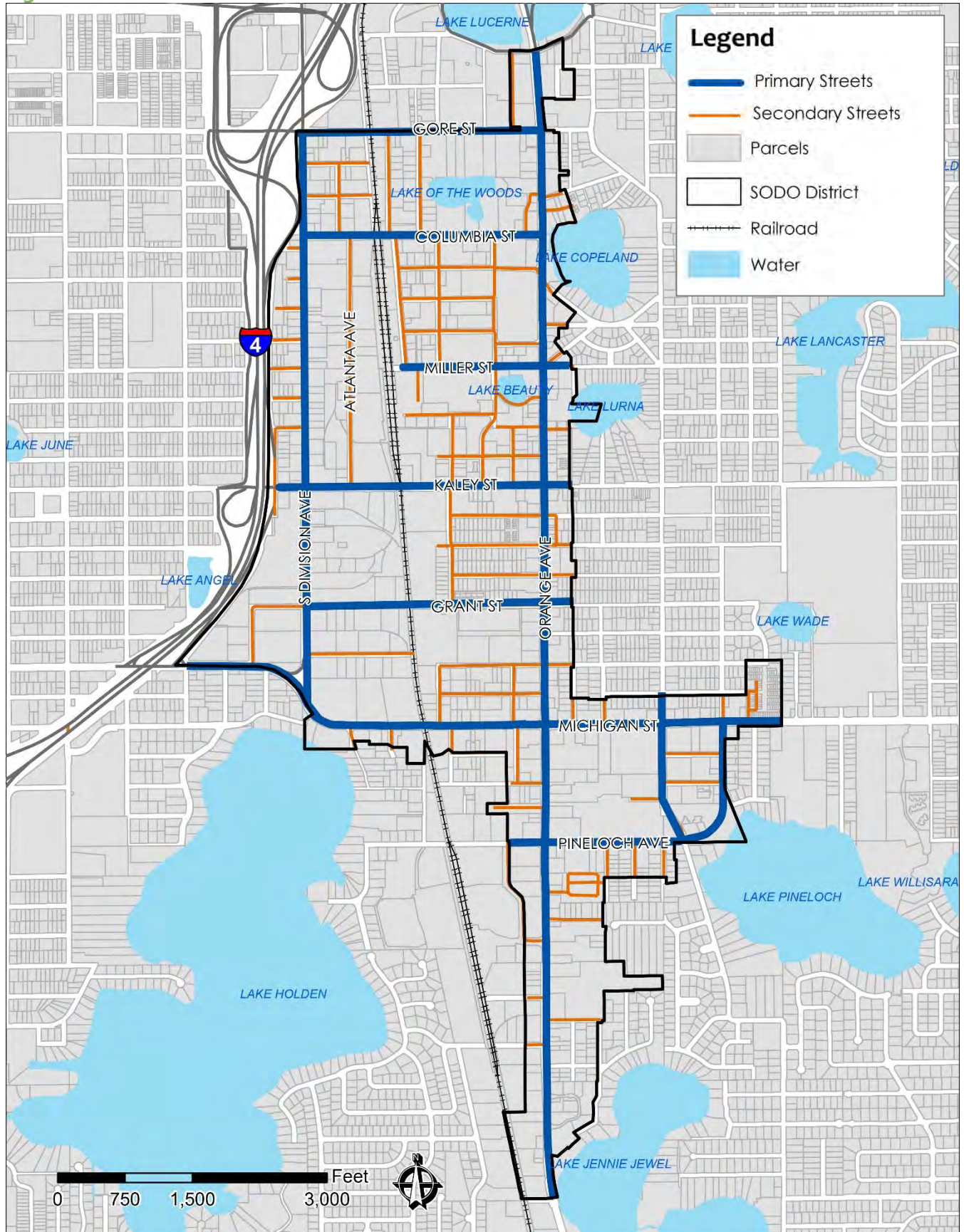
- a. Along state roads, the developer shall comply with FDOT requirements for improvements along the right-of-way.
- b. Along all other roadways, the developer shall be responsible for the provision of curbing, landscape zone (landscaping strip between the sidewalk and the travel lane) and sidewalk as specified in this document and the SODO Complete Streets Master Plan.

### **2. GENERAL STANDARDS.**

Streets in the SODO district must be designed with the primary goal of assuring pedestrian comfort and safety and increasing mobility options.

- a. All streets must be public. Private, gated, and dead-end streets and culs-de-sac are prohibited. Drives within developments designed to function as streets do not have to be dedicated as such (see Section I.5, Side Setback/ Building Frontage).
- b. The design of each street must adhere to the specifications and cross-section illustrations for each street type provided in the SODO Complete Streets Master Plan.

Figure 74: Street Classification



## P. OPEN SPACE/RECREATION REQUIREMENTS

Open space includes active and passive outdoor recreational areas designed to be used by the general public. The provision of open space areas shall not be done on a lot-by-lot basis, but rather area-wide.

Whether it is a private developer or the City providing the open space, the design of each space shall consider the following guidelines.

**Table 11. Open Space Location**

	T-3	T-4	T-5	T-6
Park		X		
Green	X	X	X	X
Square	X	X		X
Plaza	X	X		X
Playground				X

### 1. PARK.

A natural preserve available for unstructured recreation consisting of paths, trails, meadows, benches, woodland, and open shelters, all naturalistically disposed.

- Parks are often irregular in shape, but may be linear to follow natural corridors.
- Parks must front on at least one street.
- The minimum recommended size shall be 1/2 acre.



### 2. GREEN.

An open space for unstructured recreation. Greens consist of lawns, trees, paths, benches, and open shelters, all informally arranged.

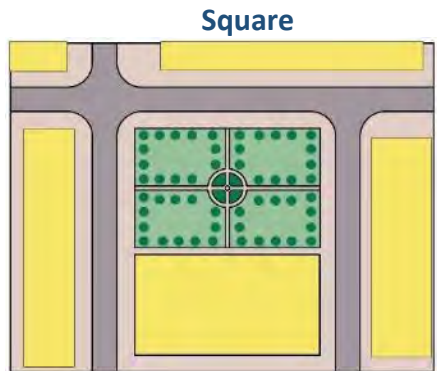
- Greens may be spatially defined by landscaping rather than building frontages.
- Greens must front on at least two streets.
- The minimum recommended size shall be 2 acres.



### 3. SQUARE.

An open space for recreation and civic purposes consisting of paths, lawns, and trees, all formally arranged. A square is spatially defined by abutting streets and building frontages.

- Squares shall be located at the intersections of important thoroughfares.
- Squares must front on at least 2 streets.
- The minimum recommended size shall be 1 acre.
- Façades facing the square should have at least forty (40) percent of their first floor's primary façade in transparent windows.

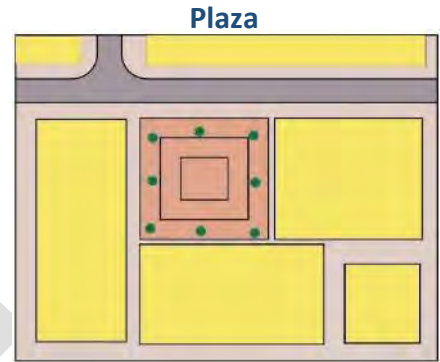




#### 4. PLAZA.

An open space for commercial and civic purposes consisting primarily of paved surfaces. A plaza is spatially defined by building frontages.

- a. Plazas shall be located at the intersections of important streets.
- b. Plazas must front on at least 1 street.
- c. The minimum recommended size shall be 4,000 square feet.
- d. Façades facing the plaza should have at least forty (40) percent of their first floor's primary façade in transparent windows.



#### 5. PLAYGROUND.

A fenced open space designed and equipped for the recreation of children.

- a. Playgrounds shall be located within walking distance of surrounding neighborhoods.
- b. Playgrounds may be freestanding or located within parks and greens.
- c. There are no minimum or maximum size requirements.

